

**CITY OF VAUGHAN**

**EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016**

Item 14, Report No. 27, of the Committee of the Whole, which was adopted without amendment by the Council of the City of Vaughan on June 28, 2016.

**14 CLASS ENVIRONMENTAL ASSESSMENT STUDY NOTICES OF COMPLETION  
PORTAGE PARKWAY WIDENING AND EASTERLY EXTENSION TO WEST  
OF BLACK CREEK (PART A); AND PORTAGE PARKWAY EXTENSION FROM WEST  
OF BLACK CREEK TO CREDITSTONE ROAD (PART B)  
WARD 4 - VAUGHAN METROPOLITAN CENTRE**

**The Committee of the Whole recommends approval of the recommendation contained in the following report of the Deputy City Manager, Planning and Growth Management and the Director of Development Engineering and Infrastructure Planning, dated June 21, 2016:**

**Recommendation**

The Deputy City Manager, Planning and Growth Management and the Director of Development Engineering and Infrastructure Planning recommend:

1. THAT staff issue Notices of Completion and place the respective Environmental Study Reports on public record for the minimum 30 day review period in accordance with the Municipal Class Environmental Assessment process for :
  - a) Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)
  - b) Portage Parkway Extension from West of Black Creek to Creditstone Road (Part B)

**Contribution to Sustainability**

Consistent with the Goals and Objectives of the City's Community Sustainability and Environmental Master Plan (Green Directions Vaughan), the Environmental Assessment Study advances and completes the planning and design process concurrently for the Portage Parkway Widening and Easterly Extension to West of Black Creek and Portage Parkway Extension from West of Black Creek to Creditstone Road, the projects will contribute to:

- transformation of the transportation network and system supporting intensification, sustainable growth and development of the Vaughan Metropolitan Centre (VMC)
- establishing a green and complete street network of sidewalks, paths and trails that support all modes of non-vehicular transportation
- developing a network of primary roads that support efficient and accessible public and private transit
- reducing single occupant vehicle trips by supporting active transportation, carpooling and public transit

**Economic Impact**

The Class Environmental Assessment Study for the Portage Parkway Widening and Easterly Extension to Creditstone Road is being funded from approved Capital Projects DT-7073-11 and DT-7071-11 with funding from City-wide Development Charges.

The estimated project cost associated with the recommended Preliminary Preferred Design for the Portage Parkway widening, extension and improvements, including engineering, construction, utility relocations and other project costs is approximately \$15.4 million as detailed below:

- \$9.2 million for Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)

## CITY OF VAUGHAN

### EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016

#### Item 14, CW Report No. 27 – Page 2

- \$6.2 million for Portage Parkway Extension from West of Black Creek to Creditstone Road including \$3.2 million for the crossing of the Black Creek channel (Part B)

Estimated project costs for the recommended Preliminary Preferred Design do not include property acquisition and are subject to adjustment and refinement through the completion of detailed design and engineering.

The Portage Parkway improvements are included as a project in the 2013 Development Charges Background Study at an estimated cost of \$14.34 million. Accordingly, the increase in the estimated project cost will be captured in the next update of the Development Charge Background Study, which is scheduled for 2018.

Funding for the completion of detailed design and engineering for the reconstruction of Portage Parkway from Applewood Crescent to Jane Street is programmed under Capital Project DT-7114-16 from City-wide Development Charges.

#### **Communication**

Key to study communication is the implementation of a comprehensive and responsive Consultation Plan, to ensure meaningful consultation with internal and external stakeholders while meeting mandatory points of contact in accordance with Municipal Class EA for Schedule C projects. The Consultation Plan, organized around study phases, included public information centres, stakeholder engagement and participation of technical review/ regulatory agencies at study milestones. The City's website, printed media and correspondence provide information on the Study's progress and notice of key Study milestones. Draft Executives Summaries, attachments 1 and 2 respectively, provide further details.

The Consultation Plan was led by the Portage Parkway EA project team comprised of CIMA+ and City staff. Integral to the Consultation Plan was internal stakeholder communication that included coordination with the VMC Project Management team with respect to ongoing and emerging projects and initiatives in the VMC.

A Technical Agencies Committee (TAC) comprised of interested representatives of regulatory and review agencies was established upon project commencement. The TAC met at key study milestones and ahead of Public Information Centres. The Consultation Plan provided for separate and focused technical meetings with approving authorities as required to review project impacts, mitigation measures and approval requirements.

A Stakeholder Group (SG) comprised of property owners/ representatives of property owners and development applicants generally within 200 metres of the Portage Parkway corridor were invited to participate. The SG met at key study milestones and ahead of Public Information Centres. The Consultation Plan provided for focused one- on-one meetings with affected property owners as required. The Portage Parkway EA project team and City staff continues to consult and actively dialogue with stakeholders and affected property owners with respect to minimizing and mitigating impacts while finalizing the Environmental Study Reports.

Notices were communicated to the public through direct mailing to a broader notice area bounded by Langstaff Road (north), Highway 407 (south), CN MacMillan Yard (east) and Weston Road (west) and by publication in the Vaughan Citizen and Thornhill Liberal.

#### **Purpose**

This report summarizes and highlights the findings and recommendations of the Class Environmental Assessment Study so that a Notice of Completion can be issued in accordance with Municipal Class Environmental Assessment process for:

## CITY OF VAUGHAN

### EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016

Item 14, CW Report No. 27 – Page 3

- Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)
- Portage Parkway Extension from West of Black Creek to Creditstone Road (Part B)

#### **Background - Analysis and Options**

#### **The City of Vaughan Transportation Master Plan (TMP) – A New Path - identified Portage Parkway Widening and Easterly Extension to Creditstone Road as a strategic improvement and key element in support of the Vaughan Metropolitan Centre**

With subway service to the Vaughan Metropolitan Centre Subway Station set to begin in late 2017 and VIVAnext Highway 7 / West Vaughan's first phase of fully dedicated Rapidway from Edgeley Boulevard to Bowes Road near completion, the timely implementation of the planned supporting street network that addresses travel demand, capacity and mobility needs of all users, with priority to transit and non-auto based modes of travel, is a key element of the transformation and building of the Vaughan Metropolitan Centre.

Council's adopted Official Plan (2010) and companion Transportation Master Plan (TMP) A New Path (2012) and focused area transportation studies, in support of the Vaughan Metropolitan Centre Secondary Plan, identified strategic road improvements to support regional initiatives and future development areas in accordance with the sustainable strategy for growth.

The TMP recommended improvements to Portage Parkway, a major collector road, as part of the Vaughan Metropolitan Centre grid network supporting intensification and transit-oriented development in Vaughan's centre and future Downtown. The easterly extension to Creditstone will provide a direct link from the industrial area east of the VMC to the planned relocated northbound on-ramp to Highway 400, thus facilitating the diversion of truck traffic from Highway 7.

#### **The TMP recommends the completion of the Municipal Class EA for the Portage Parkway Widening and Easterly Extension to Creditstone Road**

The TMP A New Path, having addressed Phases 1 and 2, recommended completing the planning and design phase (Phases 3 and 4) in accordance with the Municipal Class Environmental Assessment.

Portage Parkway is an east-west road from its westerly connection at Chrislea Road and is currently built to a 4-lane urban cross section (2 lanes in each direction) from Applewood Crescent to Edgeley Boulevard. The right-of-way through this section is approximately 26 metres. East of Edgeley Boulevard to Jane Street, Portage Parkway is currently built to 2 lane urban cross section to the terminus at the signalized intersection with Jane Street. The right-of-way through this section is approximately 23 metres. Portage Parkway west to Chrislea Road features an overpass structure crossing Highway 400 - a strategic connection in the City's transportation network and system that opened in late 2010.

The VMC Secondary Plan and focused area supporting plans and studies, in the context of broader area, provided a robust planning and design framework for advancing and completing the EA (planning and design) Study process. These principally included:

- Vaughan Metropolitan Centre Transportation Plan (June 2012) and VMC and Surrounding Areas Transportation Study (March 2013)
- Vaughan Metropolitan Centre Municipal Servicing Class Environmental Assessment Master Plan (November 2012)
- Black Creek Stormwater Optimization Study Municipal Class Environmental Assessment Master Plan (February 2012) and ongoing Black Creek Renewal EA
- VMC Streetscape and Open Space Plan (November 2015), City wide Streetscape Implementation Manual and Financial Strategy (November 2014) and Design Criteria and Standard Drawings

CITY OF VAUGHAN

**EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016**

Item 14, CW Report No. 27 – Page 4

**In May 2015, CIMA+ was retained to complete the Environmental Assessment Study, for two interrelated parts, the project limits of which were subject to refinement through the study process**

In May 2015, the City retained CIMA+ to complete the Class Environmental Assessment Study Parkway Widening and from Applewood Crescent to Jane Street (Part A) and Portage Parkway Extension from Jane Street to Creditstone Road (Part B) - as two interrelated parts.

The EA Study has proceeded in accordance with the Municipal Class Environmental Assessment 2000 as amended in 2007 & 2011 and is now substantially complete.

**CIMA and City staff, over the course of the year has substantially completed the Class EA Study and refined project limits for:**

- **Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)**
- **Portage Parkway Extension from West of Black Creek to Creditstone Road (Part B)**

The total distance of the EA Study area corridor is approximately 1.5 km. The Study and recommended Preliminary Preferred Design is supported by multi-disciplinary and comprehensive technical review and study of transportation, natural, socio-economic and built environment of the area corridor.

As the study progressed part limits were refined with Part A, which was initially planned to Jane Street, extended to west of Black Creek to include the Jane Street intersection and to provide for staging of the extension to an interim terminus of Portage Parkway at Black Creek. It is noted that for design and evaluation purposes, both Part A and Part B needed to consider the alignment immediately east of Jane Street.

Draft Environmental Study Reports for the respective Schedule C projects have been prepared documenting the planning and design process and have been provided to the Technical Agencies Committee and Stakeholders Group for review and comment. An executive summary of each study report are included as Attachments 1 and 2 to this report.

**The EA Study corroborates the need to improve and extend Portage Parkway as a multi-modal street**

The Portage Parkway widening, extension and associated improvements for a 4-lane major collector from Applewood Crescent to Creditstone Road designed as a multi modal street will:

- address travel demand and mobility needs of all users with priority to non –auto based modes of travel
- improve connections to the Vaughan Metropolitan Centre subway station (regional) and York Region Rapid Transit Bus Terminal (local) transit infrastructure
- advance the implementation of an alternate route to Highway 7 and Jane Street facilitating another route for trucks, which is a key stakeholder concern
- strengthen the grid network in support of the VMC and surrounding employment area development.

**Part A Portage Parkway Widening and Easterly Extension to West of Black Creek**

The recommended Preliminary Preferred Design for the Portage Parkway Widening and Easterly Extension to West of Black Creek is based on a reasonable range of alternatives, identification and consideration of effects on all aspects of the environment and a systematic evaluation. (See Attachment 1)

## CITY OF VAUGHAN

### EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016

#### Item 14, CW Report No. 27 – Page 5

Alternative design options were generated based on the existing street centre line as a fixed point, constraints presented by the existing stable built form on the north side and consideration to the emerging transformation of the VMC. Five alternative design concepts (including Do-Nothing) for a widening to 33 metres were generated with horizontal alignment varying amongst options.

Widening the right-of-way asymmetrically to the south, emerged as the preferred design concept carried forward in consultation with the stakeholders, technical agencies and with public input. Key findings are as follows:

- avoids and minimizes impacts to existing on-site private parking on the north side
- provides flexibility for a logical and orderly staging of the widening and improvements to Portage Parkway in step with near term transformation of the VMC
- provides for seamlessly staging and advancing near term implementation of improvements in coordination with and in the context of the VMC planned street network

The recommended Preliminary Preferred Design is comprised of the following key and functional elements:

- widen and improve Portage Parkway to a 33 metre right-of-way from comprised of the following functional elements:
  - active transportation facilitates – including cycle tracks
  - 4 general purpose lanes (2 in each direction) designed with a wider curb lane for transit vehicles and trucks in keeping with policies of the VMC
  - auxiliary centre/left turn lane facilitating vehicular travel demand, movements and operations
- improvements to existing and future signalized intersections including Applewood Crescent, Edgeley Boulevard and as coordinated with the realigned Millway Avenue.
- new municipal local road right-of way facilitating in the near term the extension from Jane Street to an interim terminus at a cul-de-sac west of the Black Creek. (It is noted that implementation of this functional segment is concurrently being advanced in the context of planning application OP.15.003 and Z.15.003 located at the south east quadrant of Jane Street and Portage Parkway)

#### **Part B Portage Parkway Extension from West of Black Creek to Creditstone Road**

The recommended Preliminary Preferred Design for easterly extension to Creditstone Road is based on a reasonable range of alternatives, identification and consideration of effects on all aspects of the environment and a systematic evaluation. (See Attachment 2)

Alternative design concepts were generated with consideration to challenges and constraints and iteratively short listed to a reasonable range of context sensitive alternative design options (for the section east of Jane Street to west of Black Creek) and were carried forward for evaluation. It is noted the extension east from Jane Street to west of the Black Creek was under Part B for the purposes of generating alternative design concepts and evaluation.

Challenges and constraints in generating alternative design concepts for a new road right-of-way included:

- Transport Canada's Grade Crossing Standards (July 2014) with respect to restrictions on the proximity of intersections and driveways to public grade crossings (all design concepts that did not meet this restriction were screened and not carried forward)
- existing stable built form/ uses and associated access and operations principally at the north east corner of Jane Street and Portage Parkway
- imminent proposed development in the VMC at the south east quadrant of Jane Street and Portage Parkway

## CITY OF VAUGHAN

### EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016

#### Item 14, CW Report No. 27 – Page 6

An option, extending east from the Jane Street intersection narrowing gradually from 33 metres to 25.7m at the Black Creek crossing and gradually widening to 33 metres easterly on the approaches to the future Maplecrete Road extension and Creditstone Road intersections, emerged as the preferred design concept carried forward in consultation with the stakeholders, technical agencies and with public input.

The crossing of the Black Creek was the subject of a separate set of alternative design options and evaluation for a preferred structure crossing of the Black Creek. It is noted that the EA Study provided for ongoing consultation, exchange of information and focused technical meetings with the Toronto Region Conservation Authority (TRCA) with respect to the crossing of the Black Creek channel that is a TRCA regulated area. Five preliminary design concepts were considered for the crossing grouped under culverts (2) and bridges (3). Culvert options not meeting flood criteria were dismissed from further consideration.

Notwithstanding that there are no current plans for active transportation facilities extending north from the VMC Secondary Plan area along the Black Creek channel, all alternative design options at the crossing consider and do not preclude the opportunity for north-south continuous pedestrian and cycling linkages consistent with Council's approved Term of Council Priorities.

Key findings for the preferred alternative are as follows:

- provides flexibility for a logical and orderly staging of the widening and improvements to Portage Parkway facilitating and in step with the longer term transformation of the VMC
- provides for seamlessly advancing near term implementation in coordination with and in the context of the VMC planned street network

The recommended Preliminary Preferred Design is comprised of the following key and functional elements:

- horizontal alignment that accommodates a new right-of-way ranging from approximately 25.7m - 33m provides for a future intersection at the future Maplecrete Road extension and curves south on the approach to the future signalized intersection at Creditstone Road
  - active transportation facilities – including cycle tracks
  - 4 general purpose lanes (2 in each direction) designed with a wider curb lane for transit vehicles and trucks taking into account the policies of the VMC Secondary Plan
  - auxiliary centre/ left turn lane facilitating vehicular travel demand, movements and operations
  - provision of a standard streetscape consistent with the City wide Streetscape Implementation Manual and Financial Strategy and the VMC Streetscape and Open Space Plan
- crossing of the Black Creek - 12 m wide ConSpan structure with consideration and not precluding opportunity for future planning of north- south continuation of pedestrian and cycling linkages in the broader context of the VMC Secondary Plan Environmental Open Space

#### **The EA Study recommends advancing a staged implementation of the Parts**

The EA Study recommends advancing an Implementation Plan giving priority to Part A, particularly in the vicinity of the mobility hub/VMC subway station. The following highlights the key components of the Implementation Plan;

- advance detailed design and engineering for the reconstruction of Portage Parkway from Applewood Crescent to Jane Street as programmed under Capital Project DT – 7114-16

## CITY OF VAUGHAN

### EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016

Item 14, CW Report No. 27 – Page 7

- program and allocate funding accordingly for Part A dovetailing ( where possible) with the ongoing and emerging detailed design of intersecting north-south VMC streets
- obtain approvals(permits, etc.) and acquire property (where necessary) in a timely manner

In addition, staff have been working with the development proponents to ensure the Portage Parkway project is protected for in the design of projects within the VMC.

#### **Filing the Environmental Study Reports for agency and public review is the next step in the study process**

The next steps in the study are as follows:

- finalize the Environmental Reports based on comments from the review, regulatory agencies and stakeholders
- issue a Notice of Study Completion and place the Environmental Study Reports for the minimum 30-day public review period (Phase 4 of the Municipal Class Environmental Assessment process)

#### **Relationship to Term of Council Service Excellence Strategy Map (2014-2018)**

The completion of the EA Study for the two projects advances Council's priorities with respect to:

- Improve municipal road network
- Continue to develop transit, cycling and pedestrian options to get around the City
- Facilitate the development of the VMC

#### **Regional Implications**

Region of York staff have participated through all phases of the EA Study as part of the formal Technical Agencies Committee and specifically as it relates to intersection design and access to Jane Street as Jane Street is under the Region's jurisdiction. The EA study recommends, in minimizing and mitigating impacts, the unavoidable need to relocate the existing off-set driveway access at the Jane Street intersection to the property on the north east quadrant of Jane Street and the Portage Parkway extension. It is recognized that all access to Jane Street is subject to the findings of the York Region's future Rapid Transit Corridor Environmental Assessment.

#### **Conclusion**

The Class EA Studies for the Portage Parkway widening and extension to Creditstone Road have now been completed and submitted to the Technical Agencies Committee and Stakeholders Group for their final review and comment. Upon completion of the review, it is recommended that the Environmental Study Reports be finalized and associated Notices of Completion be issued for the following Schedule C projects:

- Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)
- Portage Parkway Extension from West of Black Creek to Creditstone Road (Part B)

Issuing the Notice of Completion places the respective Environmental Study Reports on public record for the minimum 30 day review period in accordance with the Municipal Class Environmental Assessment process.

The City may proceed with the detailed design and construction of the road works after the Class EA studies have received final approval.

**CITY OF VAUGHAN**

**EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016**

Item 14, CW Report No. 27 – Page 8

**Attachments**

1. Portage Parkway Widening and Easterly Extension from Applewood Crescent to West of Black Creek – Draft Environmental Study Report Executive Summary
2. Portage Parkway Extension from West of Black Creek to Creditstone Road – Draft Environmental Study Report Executive Summary

**Report prepared by:**

Selma Hubjer, Manager Transportation Planning, 8674  
Marta Roias, Transportation Project Manager, 8026

(A copy of the attachments referred to in the foregoing have been forwarded to each Member of Council and a copy thereof is also on file in the office of the City Clerk.)



**CLASS ENVIRONMENTAL ASSESSMENT STUDY NOTICES OF COMPLETION  
PORTAGE PARKWAY WIDENING AND EASTERLY EXTENSION TO WEST OF BLACK CREEK  
(PART A); AND  
PORTAGE PARKWAY EXTENSION FROM WEST OF BLACK CREEK TO CREDITSTONE ROAD  
(PART B)  
WARD 4 - VAUGHAN METROPOLITAN CENTRE**

**Recommendation**

The Deputy City Manager, Planning and Growth Management and the Director of Development Engineering and Infrastructure Planning recommend:

1. THAT staff issue Notices of Completion and place the respective Environmental Study Reports on public record for the minimum 30 day review period in accordance with the Municipal Class Environmental Assessment process for :
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**Contribution to Sustainability**

Consistent with the Goals and Objectives of the City's Community Sustainability and Environmental Master Plan (Green Directions Vaughan), the Environmental Assessment Study advances and completes the planning and design process concurrently for the Portage Parkway Widening and Easterly Extension to West of Black Creek and Portage Parkway Extension from West of Black Creek to Creditstone Road, the projects will contribute to:

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**Economic Impact**

The Class Environmental Assessment Study for the Portage Parkway Widening and Easterly Extension to Creditstone Road is being funded from approved Capital Projects DT-7073-11 and DT-7071-11 with funding from City-wide Development Charges.

The estimated project cost associated with the recommended Preliminary Preferred Design for the Portage Parkway widening, extension and improvements, including engineering, construction, utility relocations and other project costs is approximately \$15.4 million as detailed below:

- \$9.2 million for Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)
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The Portage Parkway improvements are included as a project in the 2013 Development Charges Background Study at an estimated cost of \$14.34 million. Accordingly, the increase in the estimated project cost will be captured in the next update of the Development Charge Background Study, which is scheduled for 2018.

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### **Communication**

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### **Purpose**

This report summarizes and highlights the findings and recommendations of the Class Environmental Assessment Study so that a Notice of Completion can be issued in accordance with Municipal Class Environmental Assessment process for:

- Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)
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## **Background - Analysis and Options**

### **The City of Vaughan Transportation Master Plan (TMP) – A New Path - identified Portage Parkway Widening and Easterly Extension to Creditstone Road as a strategic improvement and key element in support of the Vaughan Metropolitan Centre**

With subway service to the Vaughan Metropolitan Centre Subway Station set to begin in late 2017 and VIVAnext Highway 7 / West Vaughan's first phase of fully dedicated Rapidway from Edgeley Boulevard to Bowes Road near completion, the timely implementation of the planned supporting street network that addresses travel demand, capacity and mobility needs of all users, with priority to transit and non-auto based modes of travel, is a key element of the transformation and building of the Vaughan Metropolitan Centre.

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The TMP recommended improvements to Portage Parkway, a major collector road, as part of the Vaughan Metropolitan Centre grid network supporting intensification and transit-oriented development in Vaughan's centre and future Downtown. The easterly extension to Creditstone will provide a direct link from the industrial area east of the VMC to the planned relocated northbound on-ramp to Highway 400, thus facilitating the diversion of truck traffic from Highway 7.

### **The TMP recommends the completion of the Municipal Class EA for the Portage Parkway Widening and Easterly Extension to Creditstone Road**

The TMP A New Path, having addressed Phases 1 and 2, recommended completing the planning and design phase (Phases 3 and 4) in accordance with the Municipal Class Environmental Assessment.

Portage Parkway is an east-west road from its westerly connection at Chrislea Road and is currently built to a 4-lane urban cross section (2 lanes in each direction) from Applewood Crescent to Edgeley Boulevard. The right-of-way through this section is approximately 26 metres. East of Edgeley Boulevard to Jane Street, Portage Parkway is currently built to 2 lane urban cross section to the terminus at the signalized intersection with Jane Street. The right-of-way through this section is approximately 23 metres. Portage Parkway west to Chrislea Road features an overpass structure crossing Highway 400 - a strategic connection in the City's transportation network and system that opened in late 2010.

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**In May 2015, CIMA+ was retained to complete the Environmental Assessment Study, for two interrelated parts, the project limits of which were subject to refinement through the study process**

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The EA Study has proceeded in accordance with the Municipal Class Environmental Assessment 2000 as amended in 2007 & 2011 and is now substantially complete.

**CIMA and City staff, over the course of the year has substantially completed the Class EA Study and refined project limits for:**

- **Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)**
- **Portage Parkway Extension from West of Black Creek to Creditstone Road (Part B)**

The total distance of the EA Study area corridor is approximately 1.5 km. The Study and recommended Preliminary Preferred Design is supported by multi-disciplinary and comprehensive technical review and study of transportation, natural, socio-economic and built environment of the area corridor.

As the study progressed part limits were refined with Part A, which was initially planned to Jane Street, extended to west of Black Creek to include the Jane Street intersection and to provide for staging of the extension to an interim terminus of Portage Parkway at Black Creek. It is noted that for design and evaluation purposes, both Part A and Part B needed to consider the alignment immediately east of Jane Street.

Draft Environmental Study Reports for the respective Schedule C projects have been prepared documenting the planning and design process and have been provided to the Technical Agencies Committee and Stakeholders Group for review and comment. An executive summary of each study report are included as Attachments 1 and 2 to this report.

**The EA Study corroborates the need to improve and extend Portage Parkway as a multi-modal street**

The Portage Parkway widening, extension and associated improvements for a 4-lane major collector from Applewood Crescent to Creditstone Road designed as a multi modal street will:

- address travel demand and mobility needs of all users with priority to non –auto based modes of travel
- improve connections to the Vaughan Metropolitan Centre subway station (regional) and York Region Rapid Transit Bus Terminal (local) transit infrastructure
- advance the implementation of an alternate route to Highway 7 and Jane Street facilitating another route for trucks, which is a key stakeholder concern
- strengthen the grid network in support of the VMC and surrounding employment area development.

**Part A Portage Parkway Widening and Easterly Extension to West of Black Creek**

The recommended Preliminary Preferred Design for the Portage Parkway Widening and Easterly Extension to West of Black Creek is based on a reasonable range of alternatives, identification and consideration of effects on all aspects of the environment and a systematic evaluation. (See Attachment 1)

Alternative design options were generated based on the existing street centre line as a fixed point, constraints presented by the existing stable built form on the north side and consideration to the emerging transformation of the VMC. Five alternative design concepts (including Do-Nothing) for a widening to 33 metres were generated with horizontal alignment varying amongst options.

Widening the right-of-way asymmetrically to the south, emerged as the preferred design concept carried forward in consultation with the stakeholders, technical agencies and with public input. Key findings are as follows:

- avoids and minimizes impacts to existing on-site private parking on the north side
- provides flexibility for a logical and orderly staging of the widening and improvements to Portage Parkway in step with near term transformation of the VMC
- provides for seamlessly staging and advancing near term implementation of improvements in coordination with and in the context of the VMC planned street network

The recommended Preliminary Preferred Design is comprised of the following key and functional elements:

- widen and improve Portage Parkway to a 33 metre right-of-way from comprised of the following functional elements:
  - active transportation facilitates – including cycle tracks
  - 4 general purpose lanes (2 in each direction) designed with a wider curb lane for transit vehicles and trucks in keeping with policies of the VMC
  - auxiliary centre/left turn lane facilitating vehicular travel demand, movements and operations
- improvements to existing and future signalized intersections including Applewood Crescent, Edgeley Boulevard and as coordinated with the realigned Millway Avenue.
- new municipal local road right-of way facilitating in the near term the extension from Jane Street to an interim terminus at a cul-de-sac west of the Black Creek. (It is noted that implementation of this functional segment is concurrently being advanced in the context of planning application OP.15.003 and Z.15.003 located at the south east quadrant of Jane Street and Portage Parkway)

## **Part B Portage Parkway Extension from West of Black Creek to Creditstone Road**

The recommended Preliminary Preferred Design for easterly extension to Creditstone Road is based on a reasonable range of alternatives, identification and consideration of effects on all aspects of the environment and a systematic evaluation. (See Attachment 2)

Alternative design concepts were generated with consideration to challenges and constraints and iteratively short listed to a reasonable range of context sensitive alternative design options (for the section east of Jane Street to west of Black Creek) and were carried forward for evaluation. It is noted the extension east from Jane Street to west of the Black Creek was under Part B for the purposes of generating alternative design concepts and evaluation.

Challenges and constraints in generating alternative design concepts for a new road right-of-way included:

- Transport Canada's Grade Crossing Standards (July 2014) with respect to restrictions on the proximity of intersections and driveways to public grade crossings (all design concepts that did not meet this restriction were screened and not carried forward)
- existing stable built form/ uses and associated access and operations principally at the north east corner of Jane Street and Portage Parkway
- imminent proposed development in the VMC at the south east quadrant of Jane Street and Portage Parkway

An option, extending east from the Jane Street intersection narrowing gradually from 33 metres to 25.7m at the Black Creek crossing and gradually widening to 33 metres easterly on the approaches to the future Maplecrete Road extension and Creditstone Road intersections, emerged as the preferred design concept carried forward in consultation with the stakeholders, technical agencies and with public input.

The crossing of the Black Creek was the subject of a separate set of alternative design options and evaluation for a preferred structure crossing of the Black Creek. It is noted that the EA Study provided for ongoing consultation, exchange of information and focused technical meetings with the Toronto Region Conservation Authority (TRCA) with respect to the crossing of the Black Creek channel that is a TRCA regulated area. Five preliminary design concepts were considered for the crossing grouped under culverts (2) and bridges (3). Culvert options not meeting flood criteria were dismissed from further consideration.

Notwithstanding that there are no current plans for active transportation facilities extending north from the VMC Secondary Plan area along the Black Creek channel, all alternative design options at the crossing consider and do not preclude the opportunity for north-south continuous pedestrian and cycling linkages consistent with Council's approved Term of Council Priorities.

Key findings for the preferred alternative are as follows:

- provides flexibility for a logical and orderly staging of the widening and improvements to Portage Parkway facilitating and in step with the longer term transformation of the VMC
- provides for seamlessly advancing near term implementation in coordination with and in the context of the VMC planned street network

The recommended Preliminary Preferred Design is comprised of the following key and functional elements:

- horizontal alignment that accommodates a new right-of-way ranging from approximately 25.7m - 33m provides for a future intersection at the future Maplecrete Road extension and curves south on the approach to the future signalized intersection at Creditstone Road
  - active transportation facilities – including cycle tracks
  - 4 general purpose lanes (2 in each direction) designed with a wider curb lane for transit vehicles and trucks taking into account the policies of the VMC Secondary Plan
  - auxiliary centre/ left turn lane facilitating vehicular travel demand, movements and operations
  - provision of a standard streetscape consistent with the City wide Streetscape Implementation Manual and Financial Strategy and the VMC Streetscape and Open Space Plan
- crossing of the Black Creek - 12 m wide ConSpan structure with consideration and not precluding opportunity for future planning of north- south continuation of pedestrian and cycling linkages in the broader context of the VMC Secondary Plan Environmental Open Space

### **The EA Study recommends advancing a staged implementation of the Parts**

The EA Study recommends advancing an Implementation Plan giving priority to Part A, particularly in the vicinity of the mobility hub/VMC subway station. The following highlights the key components of the Implementation Plan;

- advance detailed design and engineering for the reconstruction of Portage Parkway from Applewood Crescent to Jane Street as programmed under Capital Project DT – 7114-16

- program and allocate funding accordingly for Part A dovetailing ( where possible) with the ongoing and emerging detailed design of intersecting north-south VMC streets
- obtain approvals(permits, etc.) and acquire property (where necessary) in a timely manner

In addition, staff have been working with the development proponents to ensure the Portage Parkway project is protected for in the design of projects within the VMC.

**Filing the Environmental Study Reports for agency and public review is the next step in the study process**

The next steps in the study are as follows:

- finalize the Environmental Reports based on comments from the review, regulatory agencies and stakeholders
- issue a Notice of Study Completion and place the Environmental Study Reports for the minimum 30-day public review period (Phase 4 of the Municipal Class Environmental Assessment process)

**Relationship to Term of Council Service Excellence Strategy Map (2014-2018)**

The completion of the EA Study for the two projects advances Council's priorities with respect to:

- Improve municipal road network
- Continue to develop transit, cycling and pedestrian options to get around the City
- Facilitate the development of the VMC

**Regional Implications**

Region of York staff have participated through all phases of the EA Study as part of the formal Technical Agencies Committee and specifically as it relates to intersection design and access to Jane Street as Jane Street is under the Region's jurisdiction. The EA study recommends, in minimizing and mitigating impacts, the unavoidable need to relocate the existing off-set driveway access at the Jane Street intersection to the property on the north east quadrant of Jane Street and the Portage Parkway extension. It is recognized that all access to Jane Street is subject to the findings of the York Region's future Rapid Transit Corridor Environmental Assessment.

**Conclusion**

The Class EA Studies for the Portage Parkway widening and extension to Creditstone Road have now been completed and submitted to the Technical Agencies Committee and Stakeholders Group for their final review and comment. Upon completion of the review, it is recommended that the Environmental Study Reports be finalized and associated Notices of Completion be issued for the following Schedule C projects:

- Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)
- Portage Parkway Extension from West of Black Creek to Creditstone Road (Part B)

Issuing the Notice of Completion places the respective Environmental Study Reports on public record for the minimum 30 day review period in accordance with the Municipal Class Environmental Assessment process.

The City may proceed with the detailed design and construction of the road works after the Class EA studies have received final approval.

**Attachments**

1. Portage Parkway Widening and Easterly Extension from Applewood Crescent to West of Black Creek – Draft Environmental Study Report Executive Summary
2. Portage Parkway Extension from West of Black Creek to Creditstone Road – Draft Environmental Study Report Executive Summary

**Report prepared by:**

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Respectfully submitted,

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**Executive Summary of the Draft Environmental Study Report  
Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

## 0. Executive Summary

The City of Vaughan Transportation Master Plan (TMP), *A New Path* identified Portage Parkway Widening and Easterly Extension to Creditstone Road as a strategic improvement and key element in support of the Vaughan Metropolitan Centre and recommends completion of the Municipal Class Environmental Assessment.

This Environmental Assessment Study advances implementation of the Portage Parkway Widening and Easterly Extension to Creditstone Road in accordance with Schedule C of the Municipal Class Environmental Assessment (October 2000, as amended in 2007 & 2011).

The City of Vaughan retained CIMA+ in May 2015 to complete an Environmental Assessment (EA) study for the Portage Parkway Widening from Applewood Crescent to Jane Street (Part A) and Portage Parkway Extension from Jane Street to Creditstone Road (Part B) as two interrelated parts; the project limits of which were subject to refinement through the study process.

**Accordingly this Environmental Study Report documents the planning and design process, in accordance with the Municipal Class EA for:**

**Portage Parkway Widening and Easterly Extension to West of Black Creek.** Part A was initially defined as the widening and improvements from Applewood Crescent to Jane Street. As the study progressed, the project limits were extended easterly to west of Black Creek in order to facilitate the required staged reconstruction of the extension/ east leg of the Jane Street intersection and easterly extension of a local road to an interim terminus at cul-de-sac west of Black Creek.

**Documentation with respect to Part B is contained in a separate Environmental Study Report for Portage Parkway Extension from West of Black Creek to Creditstone Road.**

### 0.1 Background and Study Context

The City's Transportation Master Plan, *A New Path* (2013), Vaughan Metropolitan Centre (VMC) Secondary Plan and supporting focused area transportation plans and studies identified the Portage Parkway Widening and Easterly Extension to Creditstone Road as a strategic network improvement to support development within the VMC.

Portage Parkway is an east-west road from its westerly connection at Chrislea Road and is currently built to a 4-lane urban cross section (2 lanes in each direction) from Applewood Crescent to Edgeley Boulevard. The current right-of-way through this section is approximately 26 metres. East of Edgeley Boulevard to Jane Street, Portage Parkway is currently built to 2 lane urban cross section to the terminus at the signalized intersection with Jane Street. The right-of-way through this section is approximately 23 metres. Portage Parkway west to Chrislea Road features an overpass structure crossing Highway 400; a strategic connection in the City's transportation network and system that opened in late 2010.

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

The VMC Secondary Plan and focused area supporting plans and studies, in the context of broader area, provided a robust planning and design framework for advancing and completing the study EA (planning and design) Study process. These principally included:

- Vaughan Metropolitan Centre Transportation Plan (June 2012) and VMC and Surrounding Areas Transportation Study (March 2013)
- Municipal Servicing Class Environmental Assessment Master Plan (November 2012)
- Black Creek Stormwater Optimization Study Municipal Class Environmental Assessment Master Plan (February 2012) and ongoing Black Creek Renewal EA
- VMC Streetscape and Open Space Plan (November 2015) City wide Streetscape Implementation Manual and Financial Strategy (November 2014) and Design Criteria and Standard Drawings

The VMC Secondary Plan provides the following broader transportation planning and policy context for advancing and completing the planning and design of Portage Parkway:

- + Encourage the use of Portage Parkway for through movements linking to Highway 400.
- + Support the extent of the right-of-way for the Spadina Subway Line to Portage Parkway to protect for future subway extension beyond the VMC.
- + Link Creditstone Road to Portage Parkway and link both roads to Highway 7 and Jane Street to establish a bypass (i.e., Creditstone-Portage-Applewood Bypass) to assist with mitigating through movements on Highway 7 and Jane Street. This bypass would require the southerly extension of
- + Applewood Crescent from Portage Parkway.
- + Cooperate with York Region to implement rapid transit services on Jane Street.
- + Acquire right-of-way for an extension of Portage Parkway from Jane Street to Creditstone Road.

### 0.1.1 Study Area

**Figure 1** illustrates the study area for Part A and Part B. The total distance of the study area corridor is approximately 1.5 km. As the project progressed, it became clear that Part A widening had implications immediately east of Jane Street because of an existing driveway on the east side of the Jane Street/Portage Parkway intersection that would have to be relocated. Therefore, both Parts A and B considered implications of the alignment immediately east of Jane Street.

The EA study and recommended Preliminary Preferred Design is supported by a multi-disciplinary and comprehensive technical review and study of transportation, natural, socio- economic and built environment of the area corridor.

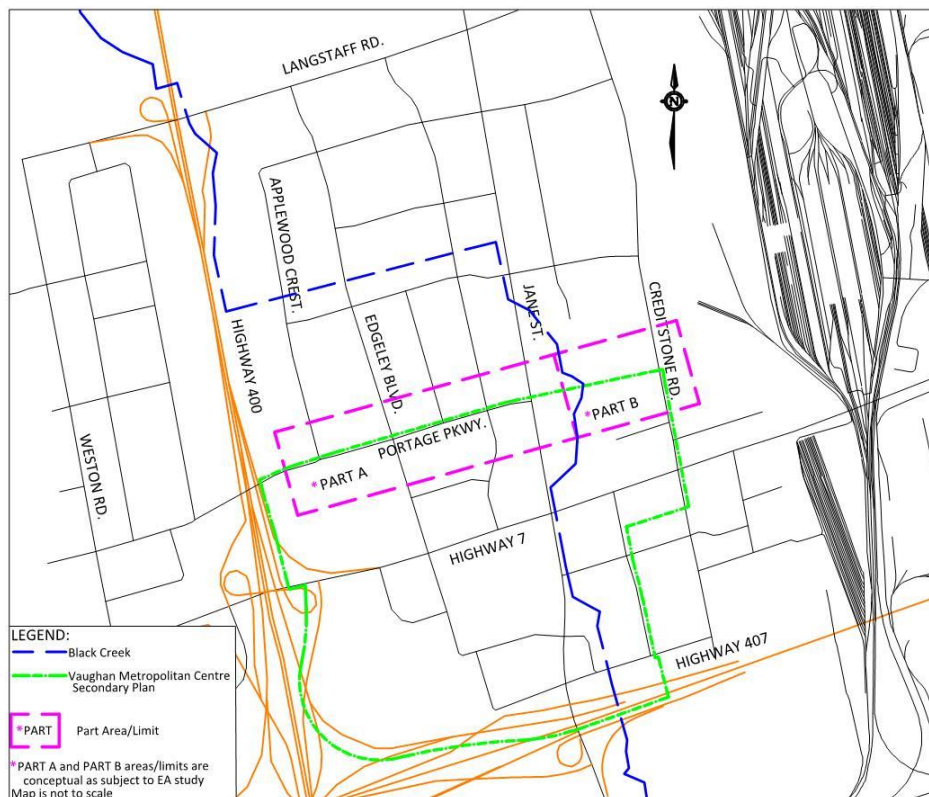
### 0.1.2 Class EA Process

Municipal projects that affect the purpose, capacity or function of a roadway, or propose new roadways, are subject to the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2007 and 2011).

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

The TMP *A New Path* (Appendix I), having addressed Phases 1 and 2, recommended completing the planning and design process in accordance with the Municipal Class EA. This EA Study revisits Phases 1 and 2 and completes Phases 3 and 4 for the Portage Parkway Widening and Easterly Extension to West of Black Creek as a **Schedule C** project under the Municipal Class EA. Schedule C projects must follow Phases 1 through 5 of the Class EA process:

- Phase 1** Identify the problem or opportunity.
- Phase 2** Identify alternative solutions to address the problem or opportunity.
- Phase 3** Examine alternative methods of implementing the preferred solution.
- Phase 4** Document the rationale for the Preferred Solution and Design Concept, and the planning, design and consultation process in an Environmental Study Report (ESR) for public and agency review.
- Phase 5** Complete contract drawings and documents, and proceed to construction, operation and environmental monitoring.



**Figure 1: Study Area for Part A and Part B**

Accordingly, separate Environmental Study Reports for Part A (and interrelated Part B) documenting Phases 1 through 4 will be placed on public record for the minimum 30 calendar days upon study completion for public and agency review. .

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

### 0.1.3 Consultation Plan

An extensive Consultation Plan, was implemented to ensure meaningful consultation with internal and external stakeholders and reviewing agencies. The Consultation Plan, organized around study phases, included public information centres, stakeholder engagement and participation of technical review/regulatory agencies at study milestones

The Consultation Plan was led by the Portage Parkway EA project team comprised of CIMA+ and City staff. The City's website, printed media and correspondence provide information on the Study's progress and notice of key Study milestones

A mailing list was developed to notify potentially interested parties of opportunities for review and comment. Four notices to the public and internal and external stakeholders were undertaken:

Notice of Commencement,	June 18 and 25, 2015
Notice of Public Information Centre No. 1,	November 10, 2015
Notice of Public Information Centre No. 2,	February 23, 2016
Notice of Completion.	Pending, 2016

To inform the general public of the study, each of the notices were advertised in two separate issues of the Vaughan Citizen and The Thornhill Liberal, and mailed to approximately 1,500 stakeholders and 93 agency representatives on the study mailing list. The Notices were also posted to the City's webpage at: [http://www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx).

The Public Information Centres (PICs) were held at the Homewood Suites Hotel near Portage Parkway on:

- + November 25, 2015 from 5:00 pm to 8:00 pm  
This PIC presented findings from Phases 1 and 2 of the project and asked for public input on the findings – 19 members of the public attended.
- + March 9, 2016 from 5:00 pm to 8:00 pm  
This PIC presented findings from Phase 3 of the project, presenting alternative designs and the preliminary preferred design, and requested public input on the findings - 14 members of the public attended.

Two advisory groups were formed and met three (3) times to provide input to the study following commencement and in advance of each PIC. Invitations to participate on the study's Technical Agencies Committee were mailed to approximately 93 agency representatives. Invitations to participate on the study's Stakeholders Group were mailed to approximately 201 property owners within 200 m north and south of Portage Parkway within the study limits.

A **Stakeholders Group** was established from interested owners of property within 200 m of the Portage Parkway study corridor, including representatives of active and proposed development in the area; and

A **Technical Agencies Committee** was established from interested representatives of regulatory and/or approving agencies on the study mailing list. Separate meetings were held with approving authorities as

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

required to review project impacts, mitigation measures and approval requirements.

One-on-one meetings were held with several land owners whose property was directly impacted by the proposed road improvements so that their concerns were considered during the evaluation of options and mitigation measures developed as appropriate. An Open House further reaching out and engaging affected property owners along the corridor was held on May 5, 2016.

The Ministry of Aboriginal Affairs advised the following First Nations may have existing or asserted rights or claims in Ontario's land claims process or litigation which may be affected by the project:

- + Chippewas of Georgina Island
- + Chippewas of Rama
- + Beausoleil First Nation
- + Mississaugas of the New Credit First Nation

## **0.2 Phase 1 – Problem and Opportunity**

The EA Study, building on the City's TMP and VMC Secondary Plan, corroborates the need to improve, and design Portage Parkway as a multi-modal street.

### **0.2.1 Needs and Justification**

A broader comprehensive traffic analysis was undertaken of the corridor that validates the need to widen Portage Parkway to 4 lanes from Applewood Crescent to Creditstone Road as a major collector road. The following highlights technical findings with respect to road way performance and operations including under a "Do-Nothing" planning scenario. :

- + Under existing traffic conditions, most intersections and turning movements operate satisfactorily during both the a.m. and p.m. peak hours;
- + Under future 2031 traffic conditions with a "Do Nothing" scenario (e.g. no widening on Portage Parkway), traffic operations along the corridor is expected to severely deteriorate, resulting in high levels of congestion; and
- + Under future 2031 traffic conditions with the proposed widening, all intersections and turning movements are expected to operate satisfactorily during the a.m. peak hour.

### **0.2.2 Problem and Opportunity Statement**

The following Problem and Opportunity Statement was developed and used to guide the remainder of the planning process:

*Transportation improvements are needed to support growth and to provide alternative truck routes to Highway 7 within the VMC.*

*Improving Portage Parkway from Applewood Crescent to Jane Street is an opportunity to:*

- + *Serve the VMC and surrounding employment area,*
- + *Create an alternative route for trucks to bypass the VMC core,*
- + *Improve connections to local and regional infrastructure,*

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

- + *Support transit-oriented nodes and corridors, and*
- + *Enhance transit ridership, cycling and walking.*

*The extension of Portage Parkway from Jane Street to Creditstone Road is an opportunity to:*

- + *Provide a continuous route for all modes of transportation from Weston Road crossing Highway 400 and the Black Creek channel to Creditstone Road, and*
- + *Alleviate traffic congestion on Highway 7 within the VMC.*

### **0.3 Phase 2 – Alternative Planning Solutions**

Six (6) alternative planning solutions were identified and evaluated as part of Phase 1 and 2 of the Class EA process:

#### **Do Nothing**

This solution would leave Portage Parkway unmodified in an 'as is' state.

#### **Travel Demand Management Initiatives**

Travel demand management initiatives involve strategies and policies used to reduce travel demand or redistribute the demand spatially or temporally.

#### **Alternative Modes of Transportation**

Promoting and facilitating the use of alternative modes of transportation, such as transit and cycling, can reduce the demand on a roadway.

#### **Localized Intersection and Operational Improvements**

Operational improvements such as the retiming of traffic signals and installation of turning lanes, can improve the overall efficiency of a roadway (i.e. maximize throughput) and the surrounding network.

#### **Widening Portage Parkway from Applewood Crescent to Jane Street**

This solution would increase the capacity of the roadway between Applewood Crescent and Jane Street.

#### **Extending Portage Parkway from Jane Street to Creditstone Road**

This solution addresses the local areas network discontinuity by extending Portage Parkway from Jane Street to Creditstone Road.

An advantage/disadvantage evaluation process was used to compare the alternatives. Two (2) of the recommended solutions have previously been recommended as part of the Transportation Master Plan (TMP) and are assumed to continue, which are:

- + Travel Demand Management Initiatives – Identified in the TMP and will be implemented by the City as a separate strategy.
- + Alternative Modes of Transportation – Identified in the TMP, including the provision for continuous sidewalks, cycling systems, connectivity of the subway extension to Highway 7, and rapid transit of Jane Street.

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

The other three (3) recommended solutions are:

- + Localized Intersection and Operational Improvements;
- + Widening Portage Parkway from Applewood Crescent to Jane Street; and
- + Extending Portage Parkway from Jane Street to Creditstone Road.

The alternative planning solutions and evaluation were presented to the Advisory Groups (Technical and Stakeholder) and also at the first PIC. Minor comments were received during this consultation process, mostly concerning requests to be kept up to date as the project proceeds.

## **0.4 Phase 3 – Alternative Designs**

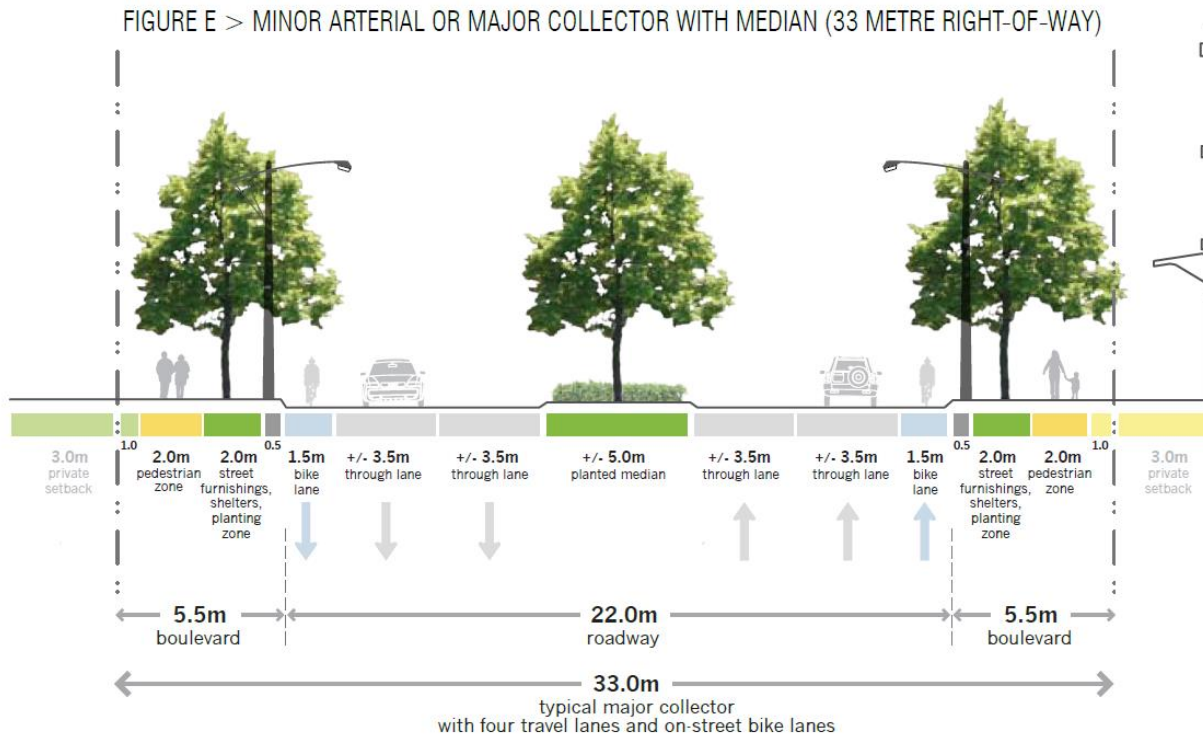
Development of the design concepts and alternative options, evaluated against technical and environmental based criteria, considered the three-dimensions of a road corridor; i.e. the cross-section, the horizontal alignment and vertical profile as follows.

### Cross-Section

The VMC Secondary Plan (Figure below) and VMC Streetscape and Open Space Plan as coordinated with the City's broader City-wide Streetscape Implementation Manual and Financial Strategy and Design Criteria and Standard Drawings provided the planning and design context and framework for the exploration of alternative typical cross-sections.

The beginning point for the development of a typical cross-section was the street cross section from the VMC Secondary Plan (figure below) and organization of streetscape infrastructure as guided by the VMC Streetscape and Open Space Plan. The symmetrical street cross section in the VMC Secondary Plan accommodates four 3.5m travel lanes, two 1.5m on-street bicycle lanes and 5.5m boulevards within a 33 metre right of way. The VMC Streetscape and Open Space Plan incorporates cycling facilities into the street network to build a cohesive and permeable network - identifying a cycle track for Portage Parkway.

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
 Class Environmental Assessment  
 City of Vaughan  
 May 27, 2016



Recognizing high truck volumes associated in large part with the surrounding industrial and employment areas, the preferred typical cross-section, shown in figure 2, was refined to provide a wider (relative to 3.3m inside lane) 3.5m curb lane, and wider raised off-road cycle track at 1.8m separated by a rolled curb facilitating trucks and cyclists respectively.



**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

### 0.4.1 Technical and Environmental Criteria

Each of the alternative design options was assessed against the following technical and environmental criteria:

#### Technical Criteria

##### *Economic*

- + Capital costs

##### *Implementation*

- + Conformity with regulatory framework.
- + Construction staging and planning.
- + Impacts on existing municipal services and utilities.

##### *Infrastructure Planning*

- + Improved road safety.
- + Opportunities for other travel modes (walking, cycling, and public transit).
- + Improving road capacity and/or traffic flow.
- + Conformity with official/secondary plans and transportation master plans.

#### Environmental Criteria

##### *Natural*

- + Impacts on avian and wildlife.
- + Encroachment onto natural areas
- + Impacts on species at risk.
- + Impacts on aquatic and watercourses.
- + Impacts on vegetation.

##### *Cultural/Social*

- + Impacts on archaeology.
- + Impacts on built heritage and cultural landscapes.
- + Air quality.
- + Compatibility with emergency services requirements.
- + Property and parking access impacts.
- + Noise impacts (post construction).

### 0.4.2 Applewood Crescent to Jane Street Widening Alternatives

Horizontal alignment constraints were presented by the existing stable built form on the north side (including parking) and consideration of the emerging transformation of the VMC on the south side; these considerations led to five alternative design concepts (including Do-Nothing) being developed with the horizontal alignment varying for each option.

As previously noted, while Part A was initially identified as being west of Jane Street, as the EA Study progressed, it became clear that Part A widening had implications immediately east of Jane Street because of an existing off-set driveway on the east side of the Jane Street/Portage Parkway intersection that would have to be relocated. Therefore options were developed that considered both Parts A and B.

Four (4) design options to widen and improve Portage Parkway from Applewood Crescent to Jane Street were developed and carried forward for evaluation. The first option aligns the “widening equally on both sides”; once the business impacts (mainly loss of parking on the north side) of this option were apparent, further options were developed that varied the alignment to the south and north giving due consideration to constraints on both the north and south sides:

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

**'Do Nothing' Option**

No widening of Portage Parkway from Applewood Crescent to Jane Street – this was used for comparative purposes in order to measure the net impacts.

**Option 1: Widen Equally on Both Sides**

Widening equally about the centreline removes a row of parking from all properties on the North side.

**Option 2: Widen to the South with No Impact on North Side**

Widening to the South avoids **all** North side parking impacts, but adversely impacts parking and loading/unloading operations on the South side.

**Option 3: Widen to the South with impact on North Side**

Holding the right-of-way at its current position on the North side adversely impacts parking within the right-of-way, as well as parking and loading/unloading operations on the South side.

**Option 4: Hold Right-of-Way on Majority of Parking Curb Lines (Both Sides) - *preferred***

Holding the right-of-way to the edge of the curb line for parking on the North and South sides minimizes property impacts on the North side (affects one North-side property which is constructed partly in the City's right-of-way).

Widening the right-of-way asymmetrically to the south - holding the future right-of-way on the majority of parking curb lines on the north and south sides to the south (Option 4), - emerged as the preferred design option carried forward in consultation with the stakeholders, technical agencies and with public input.

Key features of the preferred design are as follows:

- minimizes impacts to existing on-site private parking on the north side;
- the option is compatible with the preferred option for Part B;
- provides flexibility for a logical and orderly staging of the widening and improvements to Portage Parkway facilitating and in step with imminent and nearer term transformation of the VMC; and
- provides for seamlessly staging and advancing near term implementation of improvements in coordination with and in the context of the VMC planned street network.

Meetings were held with the Stakeholders and Agencies as well as with property owners where there was a significant property impact concern. A second Public Information Centre was also held to review the design options. There was general support for the preferred design options and some minor modifications developed to further minimize impacts wherever possible.

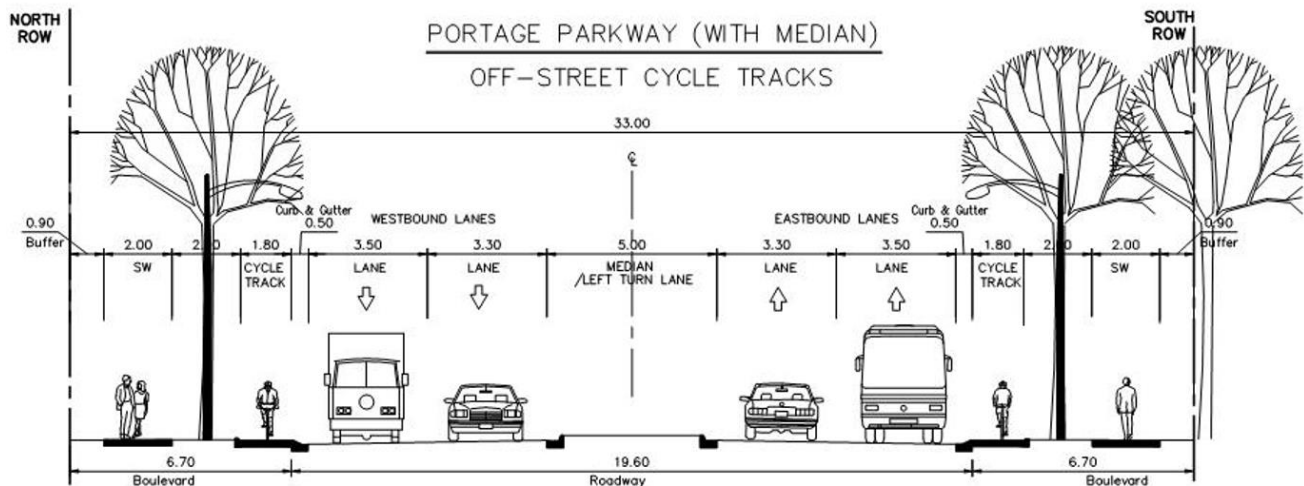
**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
 Class Environmental Assessment  
 City of Vaughan  
 May 27, 2016

## 0.5 Preliminary Preferred Design

Based on the evaluation of alternative design concepts and consultation with the affected property owners, interested public and agencies, the recommended Preliminary Preferred Design (see Plates 1- 3) is to:

- + Widen Portage Parkway from Applewood Crescent to Jane Street to 33 metre right-of –way. This preferred design minimizes impacts, improves access to several driveways through dedicated left turns, increases capacity of VMC sub-area network, improves level of service on Jane Street, and improves accommodation of active transportation.

The typical cross section for the preferred design are shown in **Figure 2**.



**Figure 2: Typical Cross-Section**

A 1.8m wide cycle track is accommodated on both sides of the road and separated from the travel lanes by a roll-over curb. A 2.0m sidewalk on each side is proposed just inside the property buffer. The south side (adjacent to the VMC) has proposed two lines of trees, one of which is on the adjacent development.

The existing vertical alignment is relatively flat. One section has a grade of less than 0.5%. The proposed vertical alignment is improved with a minimum grading of 0.5% for the entire study area.

### 0.5.1 Municipal Infrastructure/Utilities

The recommended Preliminary Preferred Design provides for the integrated and coordinated implementation of planned and approved municipal services (water and sanitary) as per the VMC Municipal Class Environmental Assessment Master Plan together with streetscape infrastructure as per VMC Streetscape and Open Space Plan and City wide Streetscape Implementation Manual and Financial Strategy

A number of utilities will require relocation to accommodate the recommended roadway design, such as:

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
 Class Environmental Assessment  
 City of Vaughan  
 May 27, 2016

- + Underground Bell cable, Rogers buried fibre, Enbridge pipe, All Stream underground ducts and cable, PowerStream underground ducts and cable.

Based on the City Standards, the proposed locations of the relocated utilities are shown in **Figure 3**.

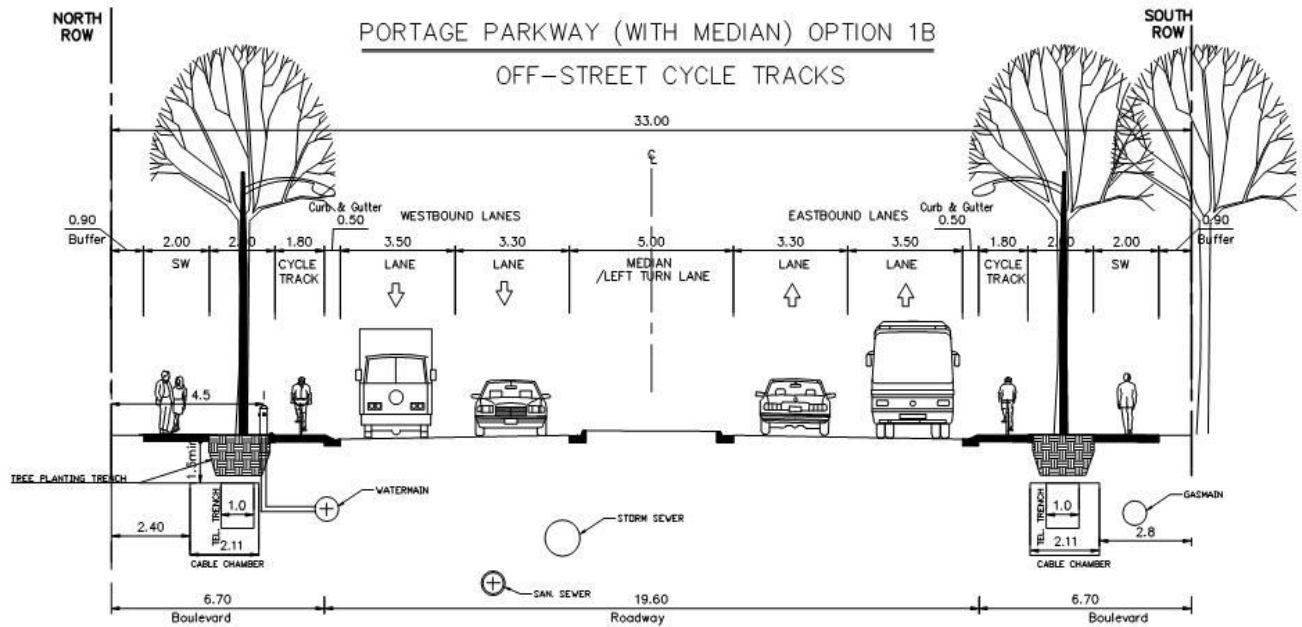


Figure 3: Typical Cross Section (with Utility Locations)

### 0.5.2 Traffic Signals

The EA Study and recommended Preliminary Preferred Design provides for staging improvements to the existing signalized intersections at Applewood Crescent and Edgeley Boulevard and implementation of a new signalized intersection at and in coordination with the realignment of Millway Avenue.

With respect to the Jane Street intersection - the EA Study considers the unavoidable need to relocate the existing off-set driveway access at the Jane Street intersection to the property on the north east quadrant of Jane Street and the Portage Parkway extension. In the regard it is noted that the EA study provided for exchange of information and ongoing dialogue with Region of York staff as part of the formal Technical Agencies Committee and specifically as it relates to intersection design and access as Jane Street is under the Region's jurisdiction. It is recognized that all access to Jane Street is subject to the findings of York Region's future Rapid Transit Corridor Environmental Assessment.

### 0.5.3 Implementation Plan

The EA Study recommends advancing staging improvements in step with the transformation of the VMC and in coordination with the VMC planned street network. The key components of the Implementation Plan follow:

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

- advance detailed design and engineering for the reconstruction of Portage Parkway from Applewood Crescent to Jane Street;
- facilitate near term improvements and extension east of Jane Street to an interim terminus at a cul-de-sac west of Black Creek;
- program and allocate funding accordingly for Part A dovetailing ( where possible) with the ongoing and emerging detailed design of intersecting north-south VMC streets; and
- obtain approvals(permits etc.) and acquire property (where necessary) in a timely manner

### **0.5.3.1 Capital Cost Estimate**

The estimated total project cost associated with the proposed improvements, including engineering, construction, utility relocations and other project costs is approximately \$9,200,000 for Part A , widening, extension and improvements for Portage Parkway from Applewood Crescent to west of Black Creek. It is noted that estimated preliminary capital cost estimates do include property acquisition and are subject to adjustment and refinement through the completion of detailed design and engineering.

### **0.5.3.2 Property Requirements**

Implementation of the recommended Preliminary Preferred Design for the widening, extension and improvements from Applewood Crescent to Jane Street will require lands from all properties on the north and south sides of Portage Parkway of up to approximately 7.7 m from the south and 3.4 m from the north. It is noted that the recommended Preliminary Preferred Design is subject to detailed design and engineering.

## **0.6 Phase 4 – Notice of Completion**

The Environmental Study Report will be placed on the public record for a minimum 30 calendar days. The Notice of Completion will announce where the report can be reviewed and will include contact information and a date for receiving comments. The Notice will further explain the process for resolving concerns. The public must contact the City of Vaughan within the 30-day review period to discuss and resolve any outstanding issues. If the issues cannot be resolved, the public may request for the Minister of Environment and Climate Change to order the City to comply with Part II of the Environmental Assessment Act, which addresses individual environmental assessments. Part II Order requests must be made to the Minister of Environment and Climate Change within the review period. Contact information for the Minister will be included in the Notice.

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

## 0. Executive Summary

The City of Vaughan Transportation Master Plan (TMP), *A New Path* identified Portage Parkway Widening and Easterly Extension to Creditstone Road as a strategic improvement and key element in support of the Vaughan Metropolitan Centre and recommends completion of the Municipal Class Environmental Assessment (EA).

This EA Study advances implementation of the Portage Parkway Widening and its Easterly Extension to Creditstone Road in accordance with Schedule C of the Municipal Class EA (October 2000, as amended in 2007 & 2011).

The City of Vaughan retained CIMA+ in May 2015 to complete an EA study for the Portage Parkway Widening from Applewood Crescent to Jane Street (Part A) and Portage Parkway Extension from Jane Street to Creditstone Road (Part B) as two interrelated parts. The project limits of which were subject to refinement through the study process.

**Accordingly this Environmental Study Report documents the planning and design process, in accordance with the Municipal Class EA for:**

**Portage Parkway Extension from West of Black Creek to Creditstone Road.**

Part B is for the extension of Portage Parkway from west of the Black Creek, crossing the Black Creek to Creditstone Road.

**Documentation with respect to Part A is contained in a separate Environmental Study Report for Portage Parkway Widening and Easterly Extension to West of Black Creek<sup>1</sup>**

### 0.1 Background and Study Context

The City's Transportation Master Plan, *A New Path* (2013), Vaughan Metropolitan Centre (VMC) Secondary Plan and supporting focused area transportation plans and studies identified the Portage Parkway Widening and Easterly Extension to Creditstone Road as a strategic network improvement to support development within the VMC.

Portage Parkway is an east-west road from its westerly connection at Chrislea Road and is currently built to a 4-lane urban cross section (2 lanes in each direction) from Applewood Crescent to Edgeley Boulevard. The current right-of-way through this section is approximately 26 metres. East of Edgeley Boulevard to Jane Street, Portage Parkway is currently built as a 2 lane urban cross section to its terminus at the signalized intersection with Jane Street. The right-of-way through this section is approximately 23 metres. Portage Parkway west to Chrislea Road features an overpass structure crossing Highway 400 – a strategic connection in the City's transportation network that opened in late 2010.

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<sup>1</sup> As the study progressed, the study area for Part A was extended easterly to the Black Creek in order to accommodate the required reconstruction of the east leg of the Jane Street intersection.

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

The VMC Secondary Plan and focused area supporting plans and studies, in the context of broader area, provided a robust planning and design framework for advancing and completing the study EA (planning and design) Study process. These principally included:

- Vaughan Metropolitan Centre Transportation Plan (June 2012) and VMC and Surrounding Areas Transportation Study (March 2013);
- Municipal Servicing Class Environmental Assessment Master Plan (November 2012);
- Black Creek Stormwater Optimization Study Municipal Class Environmental Assessment Master Plan (February 2012) and ongoing Black Creek Renewal EA
- VMC Streetscape and Open Space Plan (November 2015) City wide Streetscape Implementation Manual and Financial Strategy (November 2014) and Design Criteria and Standard Drawings

The VMC Secondary Plan provides the following broader transportation planning and policy context for advancing and completing the planning and design of Portage Parkway:

- + Encourage the use of Portage Parkway for through movements linking to Highway 400.
- + Support the extent of the right-of-way for the Spadina Subway Line to Portage Parkway to protect for future subway extension beyond the VMC.
- + Link Creditstone Road to Portage Parkway and link both roads to Highway 7 and Jane Street to establish a bypass (i.e., Creditstone-Portage-Applewood Bypass) to assist with mitigating through movements on Highway 7 and Jane Street. This bypass would require the southerly extension of
- + Applewood Crescent from Portage Parkway.
- + Cooperate with York Region to implement rapid transit services on Jane Street
- + Acquire right-of-way for an extension of Portage Parkway from Jane Street to Creditstone Road.

### 0.1.1 Study Area

**Figure 1** illustrates the study area for Part A and Part B. The total distance of the study area corridor is approximately 1.5 km. As the project progressed, it became clear that Part A widening had implications immediately east of Jane Street because of an existing driveway on the east side of the Jane Street/Portage Parkway intersection that would have to be relocated. Therefore, both Parts A and B considered implications of the alignment immediately east of Jane Street.

The EA study and recommended Preliminary Preferred Design is supported by a multi-disciplinary and comprehensive technical review and study of transportation, natural, socio- economic and built environment of the area corridor.

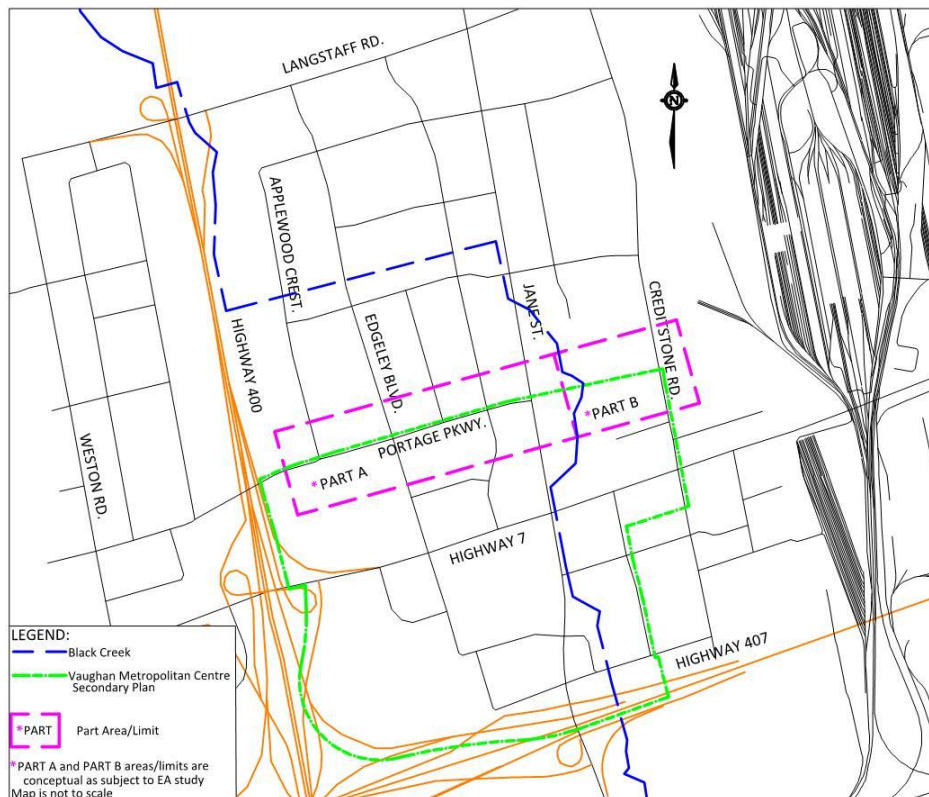
### 0.1.2 Class EA Process

Municipal projects that affect the purpose, capacity or function of a roadway, or propose new roadways, are subject to the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2007 and 2011).

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

The TMP, *New Path* (Appendix I), having addressed Phases 1 and 2, recommended completing the planning and design process in accordance with the Municipal Class EA. This EA Study revisits Phases 1 and 2 and completes Phases 3 and 4 for the Portage Parkway Widening and Easterly Extension to West of Black Creek as a **Schedule C** project under the Municipal Class EA. As noted above, Schedule C projects must follow Phases 1 through 5 of the Class EA process:

- Phase 1** Identify the problem or opportunity.
- Phase 2** Identify alternative solutions to address the problem or opportunity.
- Phase 3** Examine alternative methods of implementing the preferred solution.
- Phase 4** Document the rationale for the Preferred Solution and Design Concept, and the planning, design and consultation process in an Environmental Study Report (ESR) for public and agency review.
- Phase 5** Complete contract drawings and documents, and proceed to construction, operation and environmental monitoring.



**Figure 1: Study Area for Part A and Part B**

Accordingly, separate Environmental Study Reports for Part B (and interrelated Part A) documenting Phases 1 through 4 will be placed on public record for the minimum 30 calendar days upon study completion for public and agency review.



**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

### 0.1.3 Consultation Plan

An extensive Consultation Plan, was implemented to ensure meaningful consultation with internal and external stakeholders and reviewing agencies. The Consultation Plan, organized around study phases, included public information centres, stakeholder engagement and participation of technical review/regulatory agencies at study milestones

The Consultation Plan was led by the Portage Parkway EA project team comprised of CIMA+ and City staff. The City's website, printed media and correspondence provide information on the Study's progress and notice of key Study milestones.

A mailing list was developed to notify potentially interested parties of opportunities for review and comment. Four notices to the public and internal and external stakeholders were undertaken:

Notice of Commencement	June 18 and 25, 2015
Notice of Public Information Centre No. 1	November 10, 2015
Notice of Public Information Centre No. 2	February 23, 2015
Notice of Completion	Pending, 2016

To inform the general public of the study, each of the notices were advertised in two separate issues of the Vaughan Citizen and The Thornhill Liberal, and mailed to approximately 1,500 stakeholders and 93 agency representatives on the study mailing list. The Notices were also posted to the City's webpage at: [http://www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx).

The Public Information Centres (PICs) were held at the Homewood Suites Hotel near Portage Parkway on:

- + November 25, 2015 from 5:00 pm to 8:00 pm  
This PIC presented findings from Phases 1 and 2 of the project and asked for public input on the findings – 19 members of the public attended.
- + March 9, 2016 from 5:00 pm to 8:00 pm  
This PIC presented findings from Phase 3 of the project, presenting alternative designs and the preliminary preferred design, and requested public input on the findings - 14 members of the public attended.

Two advisory groups were formed and met three (3) times to provide input to the study following commencement and in advance of each PIC. Invitations to participate on the study's Technical Agencies Committee were mailed to approximately 93 agency representatives. Invitations to participate on the study's Stakeholders Group were mailed to approximately 201 property owners within 200 m north and south of Portage Parkway within the study limits.

A **Stakeholders Group** was established from interested owners of property within 200 m of the Portage Parkway study corridor, including representatives of active and proposed development in the area; and

A **Technical Agencies Committee** was established from interested representatives of regulatory and/or approving agencies on the study mailing list. Separate meetings were held with approving authorities as

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

required to review project impacts, mitigation measures and approval requirements.

One-on-one meetings were held with several land owners whose property was directly impacted by the proposed road improvements so that their concerns were considered during the evaluation of options and mitigation measures developed as appropriate. An Open House further reaching out and engaging affected property owners along the corridor was held on May 5, 2016.

The Ministry of Aboriginal Affairs advised the following First Nations may have existing or asserted rights or claims in Ontario's land claims process or litigation which may be affected by the project:

- + Chippewas of Georgina Island
- + Chippewas of Rama
- + Beausoleil First Nation
- + Mississaugas of the New Credit First Nation

## **0.2 Phase 1 – Problem and Opportunity**

The EA Study, building on the City's TMP and VMC Secondary Plan, corroborates the need to improve, and design Portage Parkway as a multi-modal street.

### **0.2.1 Needs and Justification**

A broader comprehensive traffic analysis of the corridor was undertaken that validates the need to widen Portage Parkway to 4 lanes from Applewood Crescent to Creditstone Road as a major collector road.

- + Under existing traffic conditions, most intersections and turning movements operate satisfactorily during both the a.m. and p.m. peak hours;
- + Under future 2031 traffic conditions with a "Do Nothing" scenario (e.g. no widening on Portage Parkway), traffic operations along the corridor is expected to severely deteriorate, resulting in high levels of congestion; and
- + Under future 2031 traffic conditions with the proposed widening and road extension, all intersections and turning movements are expected to operate satisfactorily during the a.m. peak hour.

### **0.2.2 Problem and Opportunity Statement**

The following Problem and Opportunity Statement was developed and used to guide the remainder of the planning process:

*Transportation improvements are needed to support growth and to provide alternative truck routes to Highway 7 within the VMC.*

*Improving Portage Parkway from Applewood Crescent to Jane Street is an opportunity to:*

- + *Serve the VMC and surrounding employment area,*
- + *Create an alternative route for trucks to bypass the VMC core,*

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

- + *Improve connections to local and regional infrastructure,*
- + *Support transit-oriented nodes and corridors, and*
- + *Enhance transit ridership, cycling and walking.*

*The extension of Portage Parkway from Jane Street to Creditstone Road is an opportunity to:*

- + *Provide a continuous route for all modes of transportation from Weston Road crossing Highway 400 and the Black Creek channel to Creditstone Road, and*
- + *Alleviate traffic congestion on Highway 7 within the VMC.*

### **0.3 Phase 2 – Alternative Planning Solutions**

+

Six (6) alternative planning solutions were identified and evaluated as part of Phase 1 and 2 of the Class EA process, namely:

#### **Do Nothing**

This solution would leave Portage Parkway unmodified in an 'as is' state.

#### **Travel Demand Management Initiatives**

Travel demand management initiatives involve strategies and policies used to reduce travel demand or redistribute the demand spatially or temporally.

#### **Alternative Modes of Transportation**

Promoting and facilitating the use of alternative modes of transportation, such as transit and cycling, can reduce the demand on a roadway.

#### **Localized Intersection and Operational Improvements**

Operational improvements such as the retiming of traffic signals and installation of turning lanes, can improve the overall efficiency of a roadway (i.e. maximize throughput) and the surrounding network.

#### **Widening Portage Parkway from Applewood Crescent to Jane Street**

This solution would increase the capacity of the roadway between Applewood Crescent and Jane Street.

#### **Extending Portage Parkway from Jane Street to Creditstone Road**

This solution addresses the local areas network discontinuity by extending Portage Parkway from Jane Street to Creditstone Road.

An advantage/disadvantage evaluation process was used to evaluate the appropriateness of the above alternatives. The evaluation process yielded a combination of the aforementioned alternatives as the recommended solution for Portage Parkway. Two (2) of the recommended solutions have also been recommended as part of the Transportation Master Plan (TMP), which are:

- + Travel Demand Management Initiatives – Identified in the TMP and will be implemented by the City as a separate strategy.
- + Alternative Modes of Transportation – Identified in the TMP, including the provision for continuous

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

sidewalks, cycling systems, connectivity of the subway extension to Highway 7, and rapid transit of Jane Street.

The other three (3) recommended solutions are:

- + Localized Intersection and Operational Improvements;
- + Widening Portage Parkway from Applewood Crescent to Jane Street; and
- + Extending Portage Parkway from Jane Street to Creditstone Road.

The alternative planning solutions and evaluation were presented to the Advisory Groups (Technical and Stakeholder) and also at the first PIC. Minor comments were received during this consultation process, mostly concerning requests to be kept up to date as the project proceeds.

## **0.4 Phase 3 – Alternative Designs**

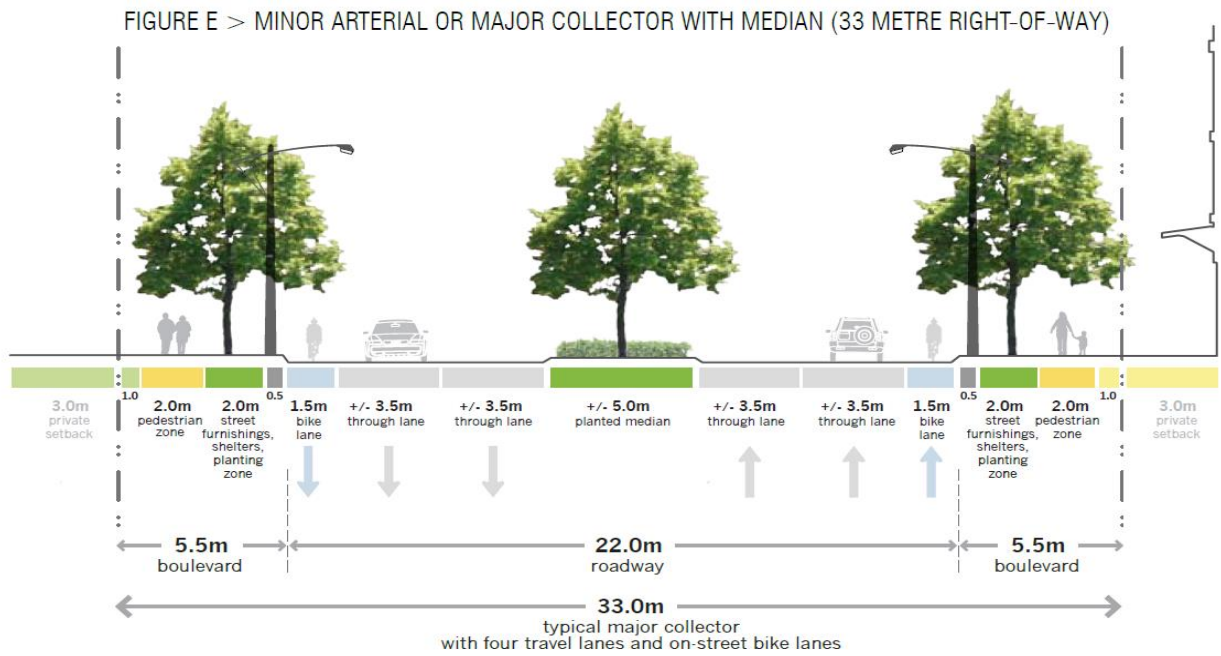
Development of the design concepts and alternative options, evaluated against technical and environmental criteria considered the three-dimensions of a road corridor; i.e. the cross-section, the horizontal alignment and vertical profile as follows.

### Cross-Section

The VMC Secondary Plan (Figure below) and VMC Streetscape and Open Space Plan as coordinated with the City's broader City-wide Streetscape Implementation Manual and Financial Strategy and Design Criteria and Standard Drawings provided the planning and design context and framework for the exploration of alternative typical cross-sections.

The beginning point for development of a typical cross-section was the street cross -sections from the VMC Secondary Plan (figure below) and organization of streetscape infrastructure as guided by the VMC Streetscape and Open Space Plan. The symmetrical street cross section in the VMC Secondary Plan accommodates four 3.5m travel lanes, two 1.5m on-street bicycle lanes and 5.5m boulevards within a 33 metre right of way. The VMC Streetscape and Open Space Plan incorporates cycling facilities into the street network to build a cohesive and permeable network, - identifying a cycle track for Portage Parkway.

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
 Class Environmental Assessment  
 City of Vaughan  
 May 27, 2016



Recognizing high truck volumes associated, in large part, with the surrounding industrial and employment areas, the preferred typical cross-section, shown in figure 2, was refined to provide a wider (relative to 3.3m inside lane) 3.5m curb lane and wider raised off-road cycle track at 1.8m separated by a rolled curb facilitating trucks and cyclists respectively. The preferred typical cross section for the crossing of Black Creek, provides for continuation of 4 general purpose lanes (2 in each direction) and a raised median within an approximate 25.7 metre right of way maintaining the wider curb lane at 3.5m and off-road cycle track at 1.8 m.

The ROW width tapered from the 25.7m width to 33m width as it approached Jane Street and Creditstone Road.

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

### 0.4.1 Technical and Environmental Criteria

Each of the alternative design options was assessed against the following technical and environmental criteria:

#### Technical Criteria

##### *Economic*

- + Capital costs

##### *Implementation*

- + Conformity with regulatory framework.
- + Construction staging and planning.
- + Impacts on existing municipal services and utilities.

##### *Infrastructure Planning*

- + Improved road safety.
- + Opportunities for other travel modes (walking, cycling, and public transit).
- + Improving road capacity and/or traffic flow.
- + Conformity with official/secondary plans and transportation master plans.

#### Environmental Criteria

##### *Natural*

- + Impacts on avian and wildlife.
- + Encroachment onto natural areas
- + Impacts on species at risk.
- + Impacts on aquatic and watercourses.
- + Impacts on vegetation.

##### *Cultural/Social*

- + Impacts on archaeology.
- + Impacts on built heritage and cultural landscapes.
- + Air quality.
- + Compatibility with emergency services requirements.
- + Property and parking access impacts.
- + Noise impacts (post construction).

### 0.4.2 Jane Street to Creditstone Road Alternatives

Alternative design concepts were generated with consideration to challenges and constraints and iteratively short listed to a reasonable range of context sensitive alternative design options and were carried forward for evaluation.

It is noted that for the extension east from Jane Street to west of the Black Creek, key challenges and constraints in generating alternative design concepts for a new road right-of-way were principally centered on:

1. An existing commercial property on the northeast corner of Jane Street and Portage Parkway and the need to relocate its driveway, potential property requirements and impacts on on-site operations;
2. A proposed development on the southeast corner of Jane Street and Portage Parkway;
3. A large property (Iron Mountain) on the northeast corner of the crossing of Black Creek;
4. Existing buildings on the southeast side of the Black Creek crossing, recognizing that they are in the VMC;
5. A CN Rail spur line from the MacMillan Yard crossing Creditstone Road and Transport Canada's

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

Grade Separated restriction with respect at least 30 metres away from this line with any new road;  
and

6. Intersection with Creditstone Road and proximity of adjacent existing driveways

In short-listing options design options, these challenges and constraints were dealt with in the following manner:

- + Item 1 and 2 above were considered in the evaluation of Part A options.
- + Item 3 was addressed by developing options that had varying degrees of impact on the Iron Mountain building.
- + Item 4, all alternatives had an impact on the buildings on the southeast side of the creek. These buildings are shown as being redeveloped in the VMC Secondary Plan. These buildings will remain until the time that the owners decide to redevelop their land.
- + Item 5 was addressed by ensuring all options were shown at least 30 metres away from the CN Rail spur.
- + Item 6 was addressed by ensuring that adjacent driveways could be accommodated in any of the designs.

Four (4) alternatives along with Do-Nothing were carried forward for evaluation and examined for the Portage Parkway road extension. The development of options proceeded in an iterative manner in avoiding and minimizing impacts, recognizing stable built form/uses and associated parking and operation:

**'Do Nothing' Option**

No extension of Portage Parkway from Jane Street to Creditstone Road – this was used for comparative purposes in order to measure the net impacts.

**Option A: Southerly Shift of alignment east of Jane Street**

This context sensitive alignment does not facilitate the proposed development on the southeast corner of the Jane Street/Portage Parkway intersection;

**Option B: Northerly Shift of alignment east of Jane Street**

This context sensitive alignment impacts property on the northeast corner of the Jane Street/Portage Parkway intersection;

**Option C: Alignment east of Jane Street with Centre Boulevard**

This context sensitive alignment impacts property on the northeast corner of the Jane Street/Portage Parkway intersection while facilitating the proposed development on the southeast corner;

**Option D: Alignment east of Jane Street without Centre Boulevard (*Preferred*)**

This context sensitive alignment minimizes impacts on the property on the northeast corner of the Jane Street/Portage Parkway intersection and facilitates proposed development on the southeast corner. This was achieved by exploring opportunities for gradually narrowing the boulevard on the north side immediately west of Black Creek.

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

Under all design options, the unavoidable need to relocate the existing off-set driveway access at the Jane Street intersection to the property on the north east quadrant of Jane Street and the Portage Parkway extension was considered. In the regard it is noted that the EA study provided for exchange of information and ongoing dialogue with Region of York staff as part of the formal Technical Agencies Committee and specifically as it relates to intersection design and access as Jane Street is under the Region's jurisdiction. It is recognized that all access to Jane Street is subject to the findings of York Region's future Rapid Transit Corridor Environmental Assessment.

### **0.4.3 Black Creek Channel Crossing**

The crossing of the Black Creek was the subject of a separate set of alternative design options and evaluation for a preferred structure crossing of the Black Creek. It is noted that the EA study provided for ongoing consultation, exchange of information and focused technical meetings with the Toronto Region Conservation Authority with respect to the crossing of the Black Creek channel as under their regulated area. Five preliminary design concepts were considered for the crossing grouped under culverts (2) and bridges (3). Culvert options not meeting flood criteria were dismissed from further consideration.

The following provides an overview of the preliminary design concepts and key considerations that emerged through the evaluation

Five (5) preliminary design concepts were considered for the crossing of the Black Creek Channel:

**Option 1: 10.6 m wide Box Culvert with two 4.5m wide culverts for multiuse trails on both sides of the creek;**

**Option 2: Single Span 35m Bridge with multiuse trails accommodated underneath the bridge;**

**Option 3: 2 Span (60m) Bridge with multiuse trails accommodated underneath the bridge;**

**Option 4: 3 Span (60m) Bridge with multiuse trails accommodated underneath the bridge;**

**Option 5: 12m wide ConSpan Culvert with two 4.5m wide culverts for multiuse trails on both sides of the creek (*Preferred*)**

As previously noted a key stakeholder for the crossing was the Toronto and Region Conservation Authority (TRCA) with various flood accommodation criteria needing to be met – two meetings were held with TRCA staff to discuss the crossing options. Option 1 for a culvert did not meet the TRCA flood criteria and was not short-listed while the other options met or exceeded the criteria.

For each of the bridge options, there is in the order of approximately a \$6 million premium relative to costs for culvert design options.

Notwithstanding that there are no current plans for active transportation facilities extending north from the VMC Secondary Plan area along the Black Creek channel, all alternative design options at the crossing consider and do not preclude the opportunity for north-south continuous pedestrian and cycling linkages.

### **0.4.4 Meetings with Stakeholders**

Meetings were held with the Stakeholders and Agencies as well as with property owners where there was a significant property impact concern. A second Public Information Centre was also held to review the



**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

design options. There was general support for the preferred design options and some minor modifications developed to further minimize impacts wherever possible.

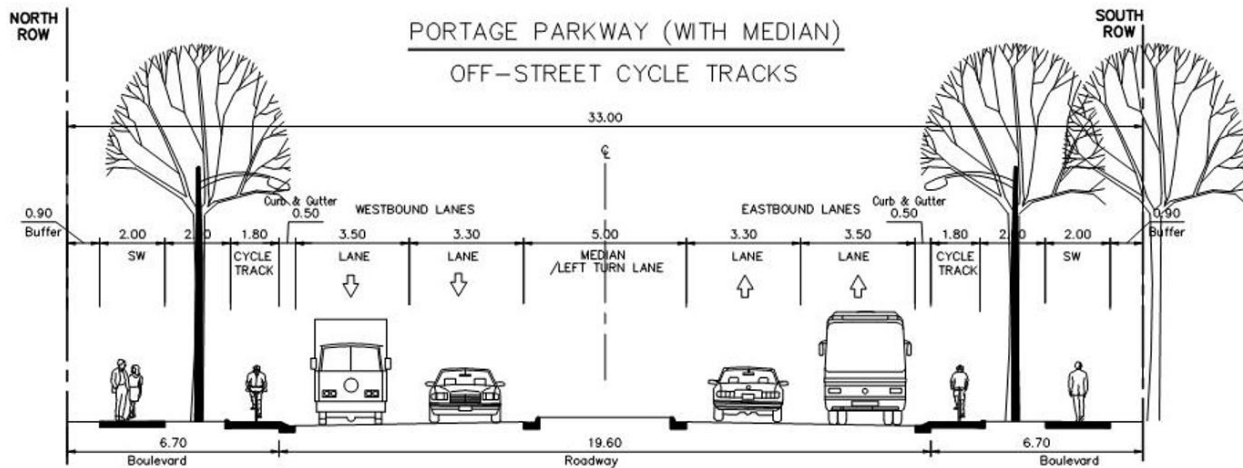
## 0.5 Preliminary Preferred Design

Based on the evaluation of alternative design concepts and consultation with the affected property owners, interested public and agencies, the recommended Preliminary Preferred Design (See Plate 1) is to:

- + Extend Portage Parkway from West of Black Creek to Creditstone Road, as per the Preliminary Preferred Design/ via an alignment as documented in the Portage Parkway Widening and Easterly Extension to West of Black Creek .
- + Accommodate the crossing of Black Creek by implementing a 12m wide ConSpan structure and provide and protect for opportunities for active transportation in the form of two separate 4.5m wide culverts.

The typical cross sections for the preferred design are shown in

**Figure 2.**



**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
 Class Environmental Assessment  
 City of Vaughan  
 May 27, 2016

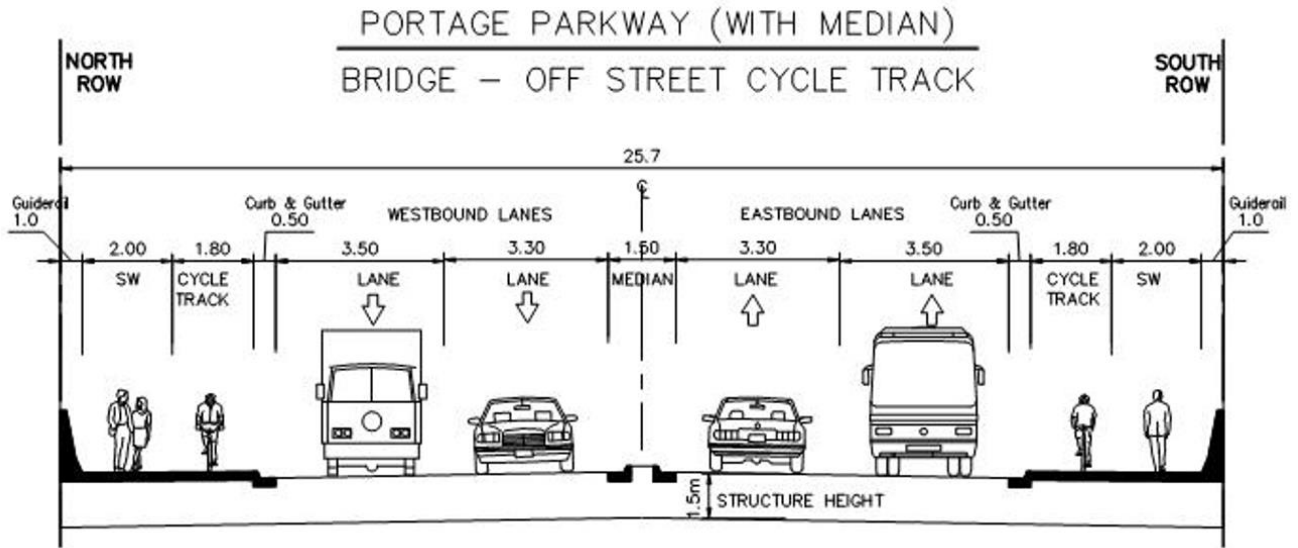


Figure 2: Typical Cross Sections

A 1.8m wide cycle track and 2.0m sidewalk are accommodated on both sides of the road and separated from the travel lanes by a rolled curb.

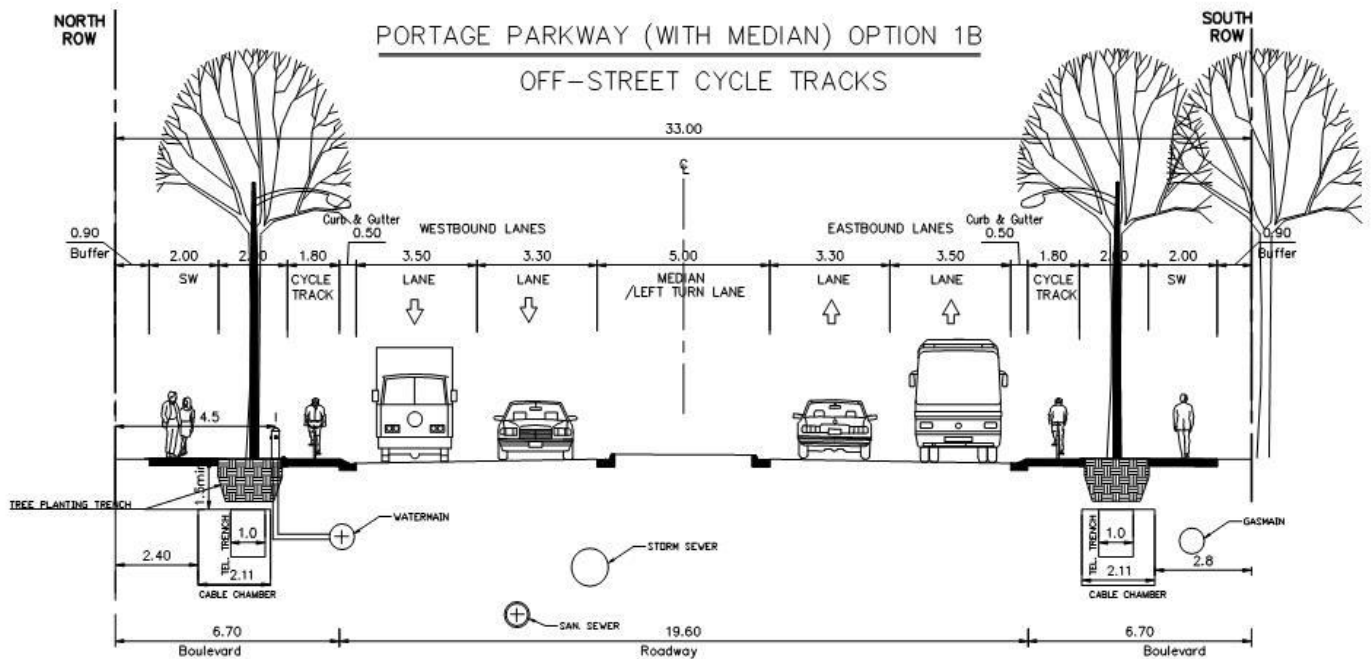


Figure 3: Typical Cross Section (with Utility Locations)

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

### **0.5.1 Municipal Infrastructure/Utilities**

The recommended Preliminary Preferred Design provides for the integrated and coordinated implementation of planned and approved municipal services (water and sanitary ) as per the VMC Municipal Class Environmental Assessment Master Plan together with streetscape infrastructure as per VMC Streetscape and Open Space Plan and City wide Streetscape Implementation Manual and Financial Strategy

The profile had to match the existing profiles at Jane Street and Creditstone Road while in between those terminal points, the need for fill in the valley was minimized by using a minimum -0.55% slope approach from Jane Street.

Based on the City Standards, the proposed locations of the relocated utilities are shown in **Figure 3** (see above).

### **0.5.2 Traffic Signals**

The EA Study and recommended Preliminary Preferred Design provides for signalized intersections at the future Maplecrete Road extension and at Creditstone Road.

### **0.5.3 Implementation Plan**

The EA Study recommends advancing an Implementation Plan staging improvements in step with the transformation of the VMC and in coordination with the VMC planned street network. The following highlights the key components of the Implementation Plan;

- program and allocate funding for completion of detailed design and engineering for the construction of the crossing of the Black Creek and extension from Creditstone Road.
- obtain approvals (permits – Black Creek Crossing, etc.) and acquire property (where necessary) in a timely manner.

### **0.5.4 Property Requirements**

Implementation of the Portage Parkway, will require the acquisition of lands from several properties including two (2) properties on the south side of Portage Parkway in the VMC Secondary Plan area.

### **0.5.5 Capital Cost Estimate**

The estimated total project cost associated with the proposed improvements, including engineering, construction, utility relocations and other project costs is approximately:

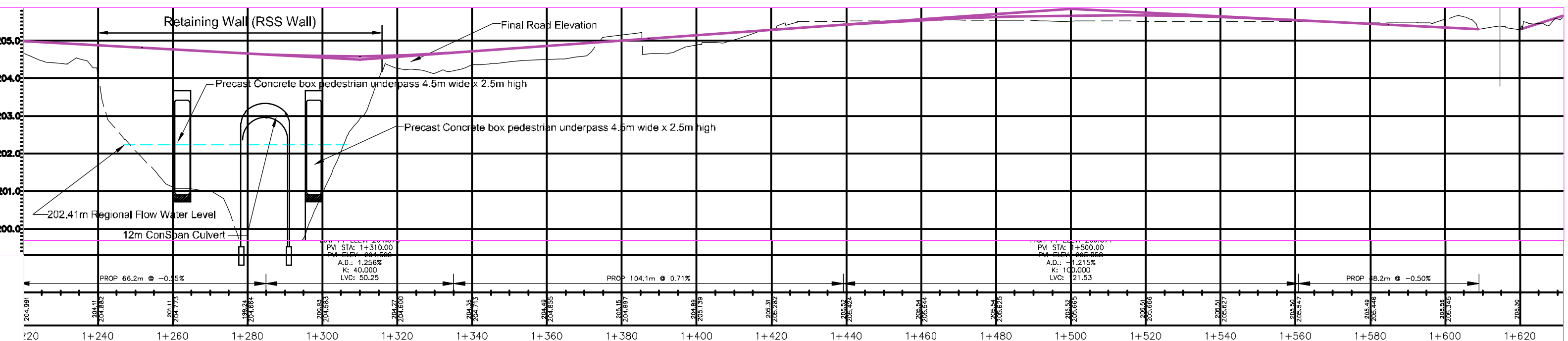
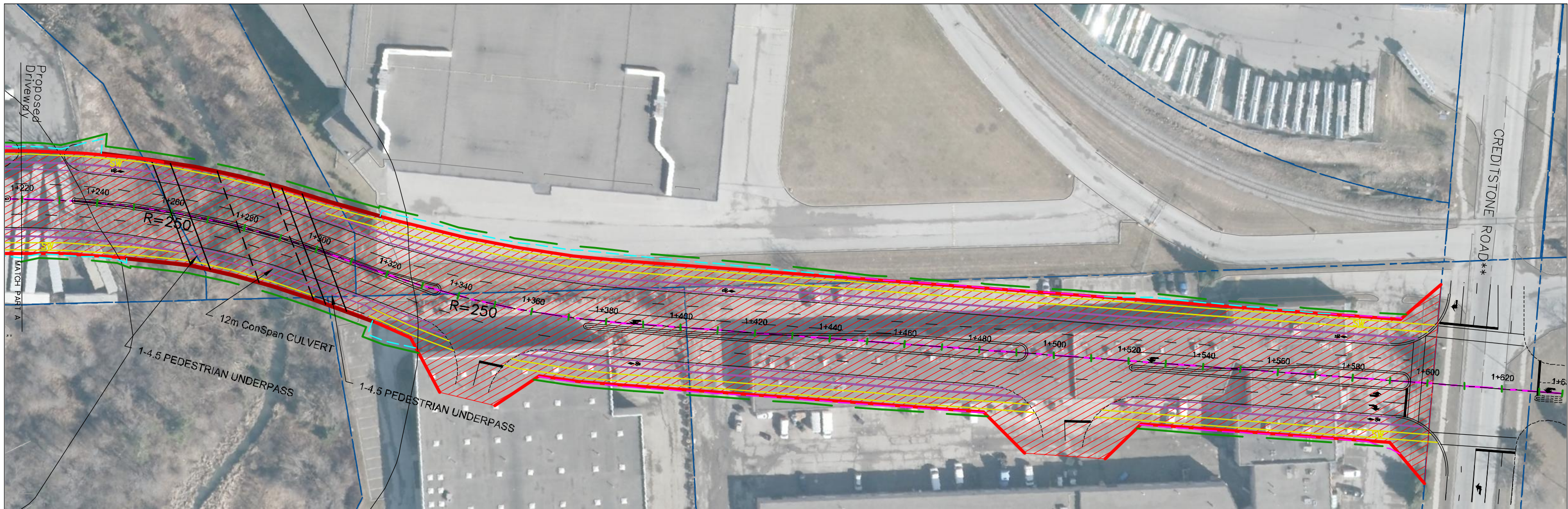
- ✦ \$6,200,000 for Part B, extending Portage Parkway from West of Black Creek to Creditstone Road (including \$3,200,000 for the Black Creek Crossing; a 12 m ConSpan culvert and provide and two separate 4.5m wide culverts on either side of the main culvert protecting and providing for opportunities for active transportation).

It is noted that estimated preliminary capital cost estimates do include property acquisition and are subject to adjustment and refinement through the completion of detailed design and engineering.

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

## **0.6 Phase 4 – Notice of Completion**

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Portage Parkway Class EA  
Preliminary Preferred Plan with Profile  
Plate 1 of 1

SCALE: 1:1000

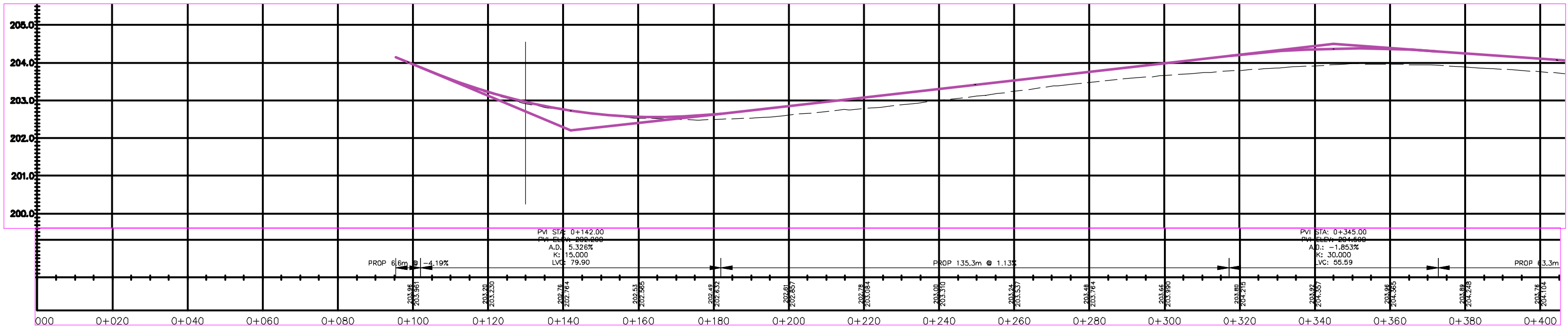
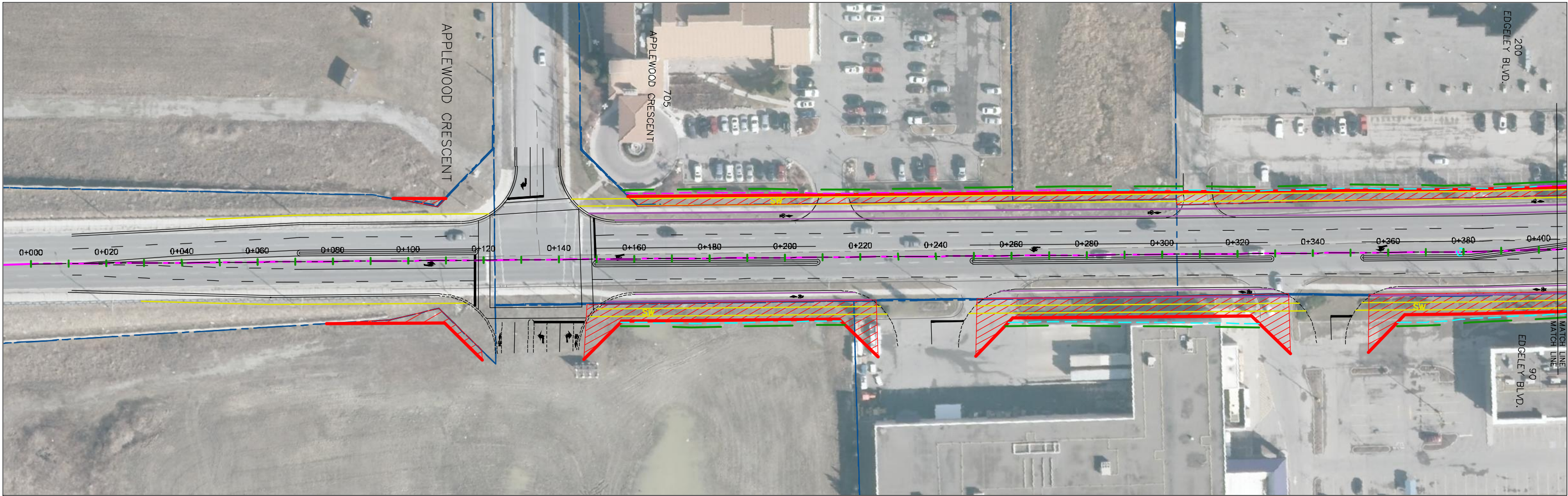
NOTE: SUBJECT TO DETAILED DESIGN AND ENGINEERING

LEGEND:

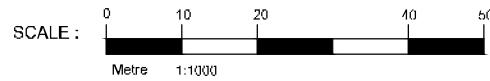
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	PROPOSED CYCLE TRACK		RETAINING WALL
	PROPOSED CURB/GUTTER		CONSTRUCTION EASEMENT LIMIT
	PROPOSED PROPERTY LINE		Relocated Signal Head
	EXISTING PROPERTY LINE		Relocated Signal Pole
	PROPERTY REQUIREMENTS		
	GRADING LIMIT - CUT		

JANE STREET\* - Subject to future York Region Rapid Transit Corridor FA Study  
CREDITSTONE ROAD\*\* - Subject to future City of Vaughan EA Study





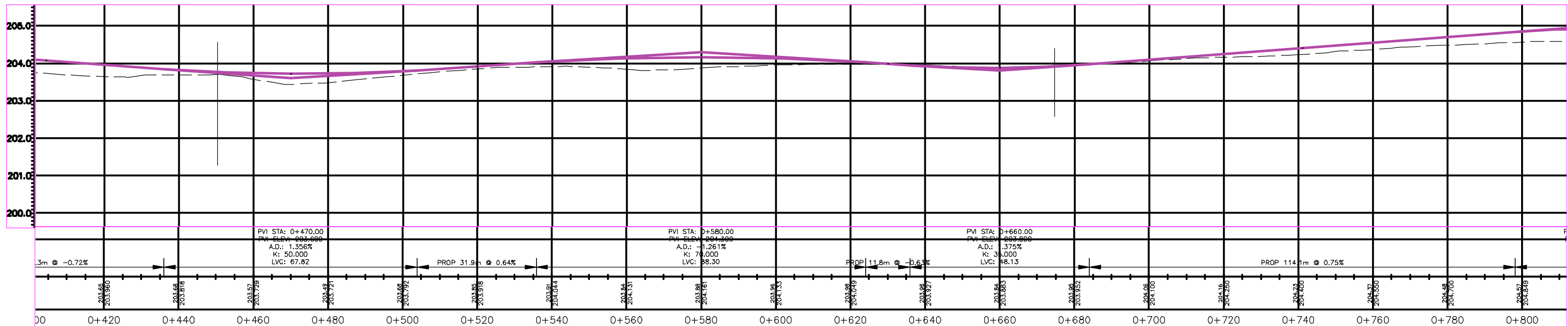
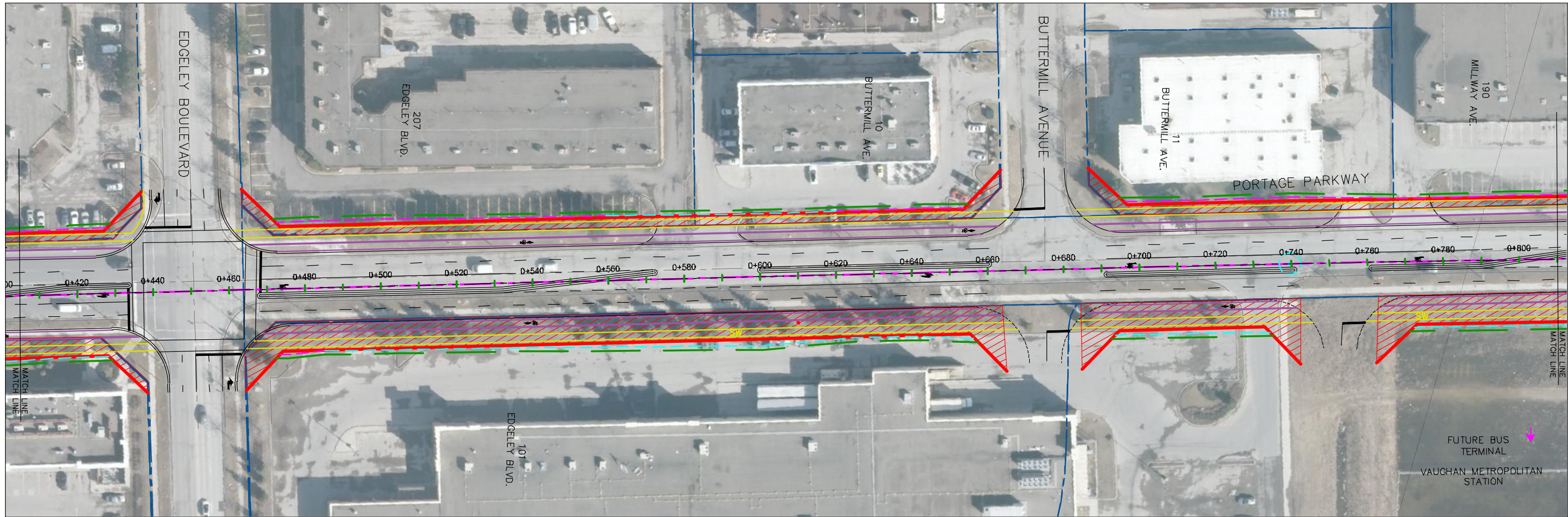
Portage Parkway Class EA  
 Preliminary Preferred Plan with Profile  
 Plate 1 of 3



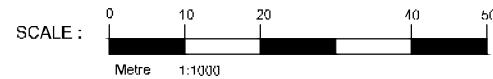
NOTE: SUBJECT TO DETAILED DESIGN AND ENGINEERING

- LEGEND:
- ▬▬▬▬▬ PROPOSED SIDEWALK
  - ▬▬▬▬▬ PROPOSED CYCLE TRACK
  - ▬▬▬▬▬ PROPOSED CURB/GUTTER
  - ▬▬▬▬▬ PROPOSED PROPERTY LINE
  - ▬▬▬▬▬ EXISTING PROPERTY LINE
  - ▨▨▨▨▨ PROPERTY REQUIREMENTS
  - ▬▬▬▬▬ GRADING LIMIT - CUT
  - ▬▬▬▬▬ GRADING LIMIT - FILL
  - ▬▬▬▬▬ RETAINING WALL
  - ▬▬▬▬▬ CONSTRUCTION EASEMENT LIMIT
  - ◀ Relocated Signal Head
  - Relocated Signal Pole
- JANE STREET\* - Subject to future York Region Rapid Transit Corridor FA Study  
 CREDITSTONE ROAD\*\* - Subject to future City of Vaughan EA Study





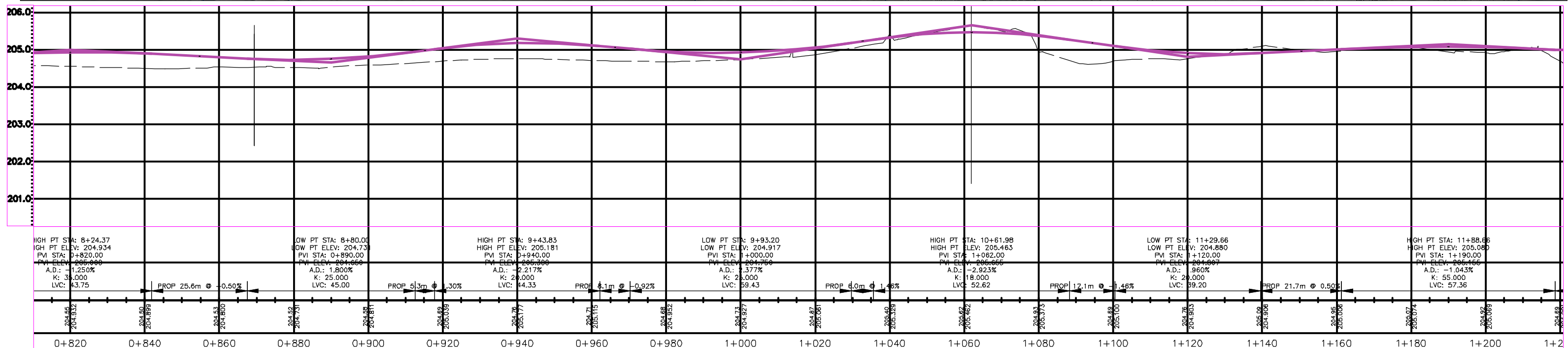
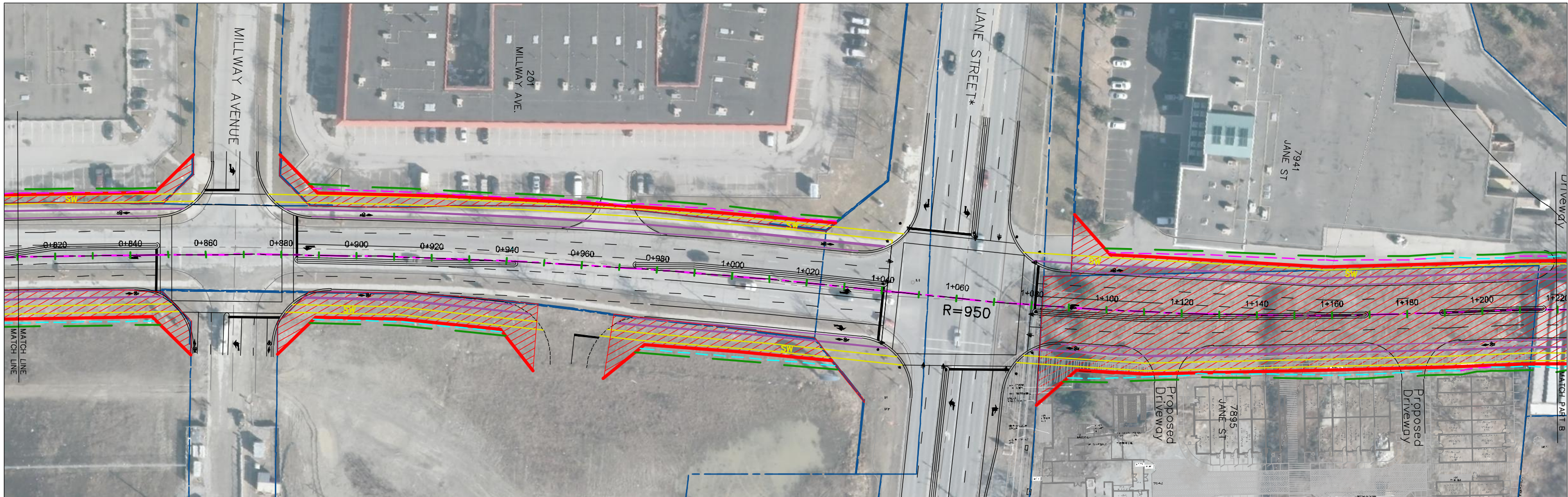
Portage Parkway Class EA  
 Preliminary Preferred Plan with Profile  
 Plate 2 of 3



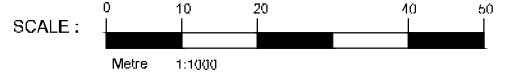
NOTE: SUBJECT TO DETAILED DESIGN AND ENGINEERING

- LEGEND:
- PROPOSED SIDEWALK
  - PROPOSED CYCLE TRACK
  - PROPOSED CURB/GUTTER
  - PROPOSED PROPERTY LINE
  - EXISTING PROPERTY LINE
  - PROPERTY REQUIREMENTS
  - GRADING LIMIT - CUT
  - GRADING LIMIT - FILL
  - RETAINING WALL
  - CONSTRUCTION CASSEMENT LIMIT
  - Relocated Signal Head
  - Relocated Signal Pole
  - JANE STREET\* - Subject to future York Region Rapid Transit Corridor FA Study
  - CREDITSTONE ROAD\*\* - Subject to future City of Vaughan EA Study





Portage Parkway Class EA  
 Preliminary Preferred Plan with Profile  
 Plate 3 of 3



NOTE: SUBJECT TO DETAILED DESIGN AND ENGINEERING

- LEGEND:
- PROPOSED SIDEWALK
  - PROPOSED CYCLE TRACK
  - PROPOSED CURB/GUTTER
  - PROPOSED PROPERTY LINE
  - EXISTING PROPERTY LINE
  - ▨▨▨▨ PROPERTY REQUIREMENTS
  - GRADING LIMIT - CUT
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