# THE CITY OF VAUGHAN

# **BY-LAW**

#### BY-LAW NUMBER 243-2024

#### A By-law to amend City of Vaughan By-law 001-2021, as amended.

**WHEREAS** there has been an amendment to the Vaughan Official Plan adopted by Council but not approved at this time, with which the matters herein set out are in conformity;

**AND WHEREAS** subsection 24(2) of the *Planning Act*, R.S.O. 1990, c.P.13 provides that Council may pass a by-law that does not conform to the Official Plan on lands that are subject to an adopted amendment, and that once the amendment comes into effect, the By-law shall then conform;

**AND WHEREAS** subsection 24(2.1) of the *Planning Act*, R.S.O. 1990, c.P.13 provides that the By-law comes into force and effect upon the Official Plan Amendment coming into effect;

**NOW THEREFORE** the Council of The Corporation of the City of Vaughan ENACTS AS FOLLOWS:

- That City of Vaughan By-law Number 001-2021, as amended, be and it is hereby further amended by
  - a) Rezoning the lands shown as "Subject Lands" on Schedule "1" attached hereto from FD Future Development Zone with site-specific Exception 1110, to EMU Employment Commercial Mixed-Use Zone, EM1 Prestige Employment Zone, EP Environmental Protection Zone and OS1 Public Open Space Zone, in the manner shown on the said Schedule "1".
  - b) Adding a new Part 14 Exception Zone with a new Subsection, being Subsection 14.1192 as follows:

Exception Number 14.1192	Municipal Address: 11421, 11455 Weston Road
Applicable Parent Zone: EMU, EM1, EP	Legal Description: Part of Lot 30, Concession 5, Parts 1, 2, 5, 7, 8, 65R- 39552, Part 1, 65R-39806
Schedule A Reference: 223, 243, 244	
By-law 243-2024	

## 14.1192.1 Permitted Uses

The following additional <u>uses</u> shall be permitted on the lands <u>zoned</u> EMU Employment Commercial Mixed-Use Zone, as shown on Figure E-1766:

- a. <u>Car Wash</u> except along Kirby Road frontage
- b. Drive-Through
- c. <u>Fueling Station</u> except along Kirby Road frontage
- d. Garden Centre accessory to a Supermarket
- e. <u>Motor Vehicle Sales</u> that do not include the outdoor display or storage of motor vehicles
- f. Research and Development
- g. <u>Shopping Centre</u> with individual retail units not exceeding a maximum gross floor area of 2,800 m<sup>2</sup>
- h. The maximum permitted gross floor area of a unit along the Kirby Road frontage shall be 929 m<sup>2</sup>
- i. Note 2 to Table 8-2 shall not apply

The following additional <u>uses</u> shall be permitted on the lands <u>zoned</u> EM1 Prestige Employment Zone, as shown on Figure E-1766:

- j. Banquet Hall in a single unit building
- k. Commercial Storage
- I. <u>Contractor's Establishment</u> excluding <u>Outdoor Storage</u>
- m. Health and Fitness Centre
- n. Motor Vehicle Body Repair
- o. Motor Vehicle Rental excluding outdoor display or storage
- p. Motor Vehicle Repair
- q. <u>Motor Vehicle Sales</u> excluding the outdoor display or storage of motor vehicles, mechanical repairs and/or autobody repair
- r. Place of Assembly
- s. Place of Entertainment excluding theatre or concert hall
- t. One (1) <u>Restaurant, Take-out</u> having a maximum floor area of 185 m<sup>2</sup>

#### u. <u>Service or Repair Shop</u>

The following <u>uses</u> shall be permitted on the lands <u>zoned</u> EP Environmental Protection Zone, as shown on Figure E-1766:

### v. Erosion and Flood Control Infrastructure

14.1192.2 Lot and Building Requirements

- 1. The following <u>lot</u> and <u>building</u> requirements shall apply to the lands <u>zoned</u> EMU Employment Commercial Mixed-Use Zone as shown on Figure E-1766:
  - a. The minimum front yard shall be 5.0 m.
  - b. The minimum <u>exterior side yard</u> shall be 3.0 m.
  - c. The minimum ground floor height shall be 4.5 m.
  - d. The required <u>build-to zone</u> shall be 3.5 m to 10.0 m.
  - e. The minimum <u>landscape</u> strip abutting a <u>street line</u> shall be 3.5 m.
  - f. Note 7 to Table 8-3 shall not apply to a surface <u>parking area</u> proposed within an <u>exterior side yard</u>.
- 2. The following lot and building requirements shall apply to the lands zoned EM1 Prestige Employment Zone as shown on Figure E-1766:
  - a. The minimum <u>front yard</u> shall be 3.5 m.
  - b. The minimum <u>exterior side yard</u> shall be 3.0 m.
  - c. The minimum <u>rear yard</u> shall be 3.0 m.

# 14.1192.3 Other Provisions

1. For the purposes of this exception, the following definition shall apply:

Erosion and Flood Control Infrastructure means a stormwater management facility outfall channel.

#### 14.1192.4 Figures

- E-1766
- c) Removing the Subject Lands on Schedule "1" from Exception 14.1110.
- d) Adding Schedule "E-1766" attached hereto as Schedule "1".
- e) Deleting Schedule A, Maps 223, 243, and 244 and substituting therefore the Schedule A, Maps 223, 243, and 244 attached hereto as Schedule "2",

Schedule "3", and Schedule "4".

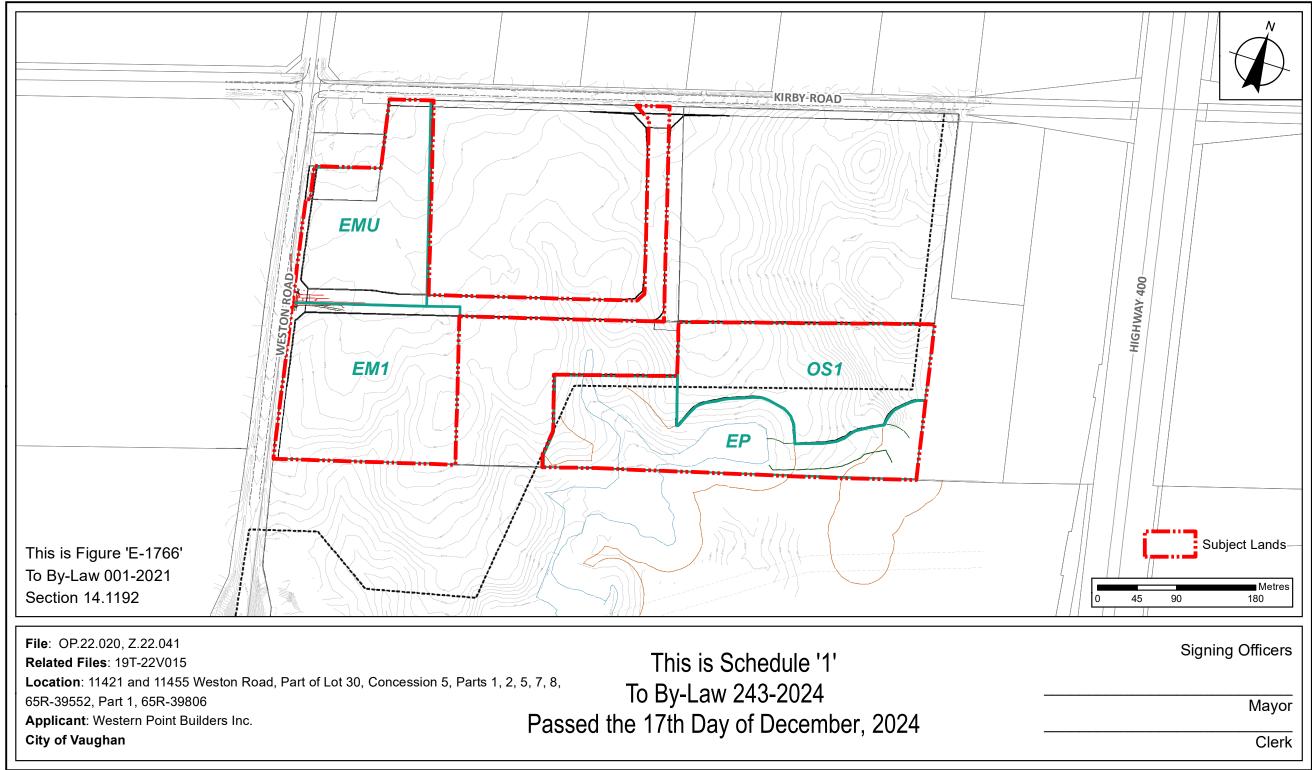
2. Schedules "1", "2", "3" and "4" shall be and hereby form part of this By-law.

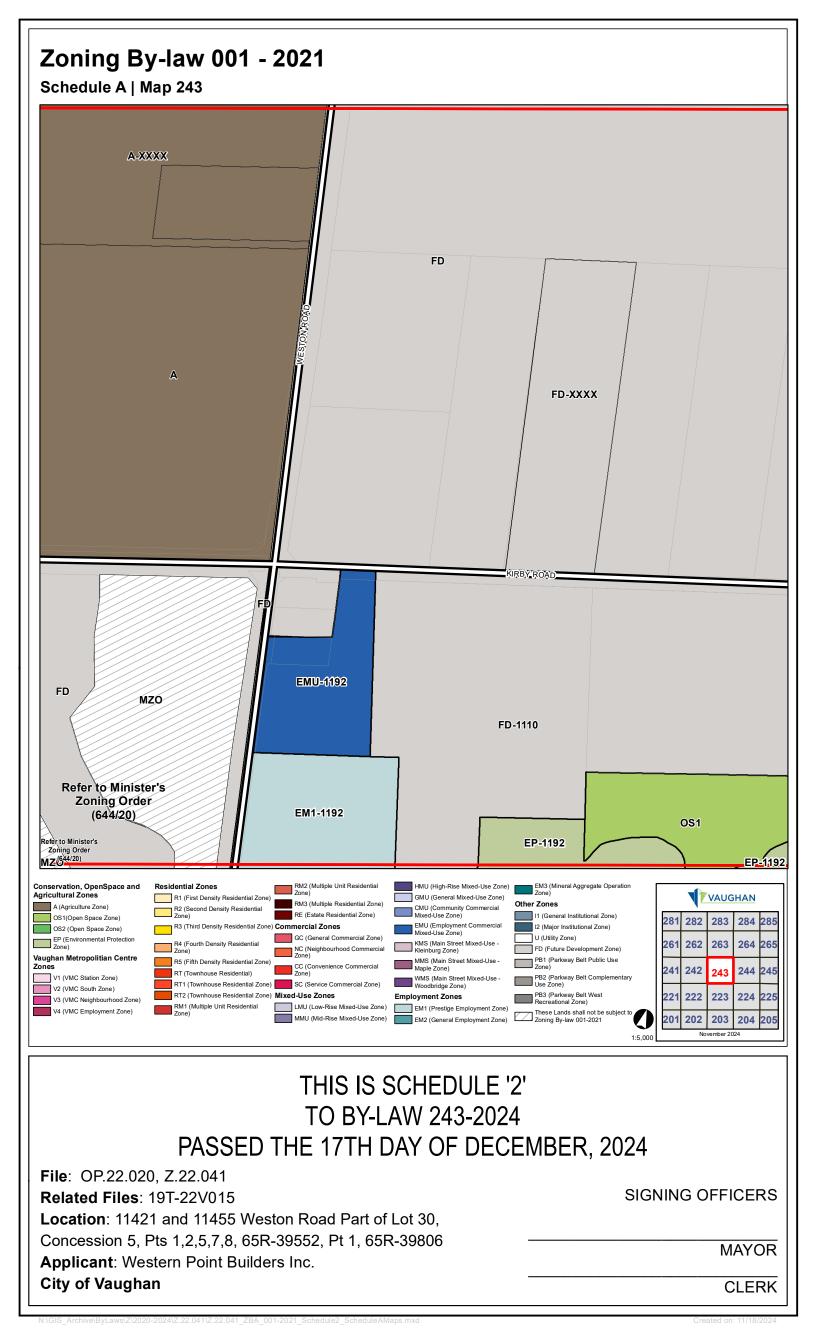
Voted in favour by City of Vaughan Council this 17<sup>th</sup> day of December, 2024.

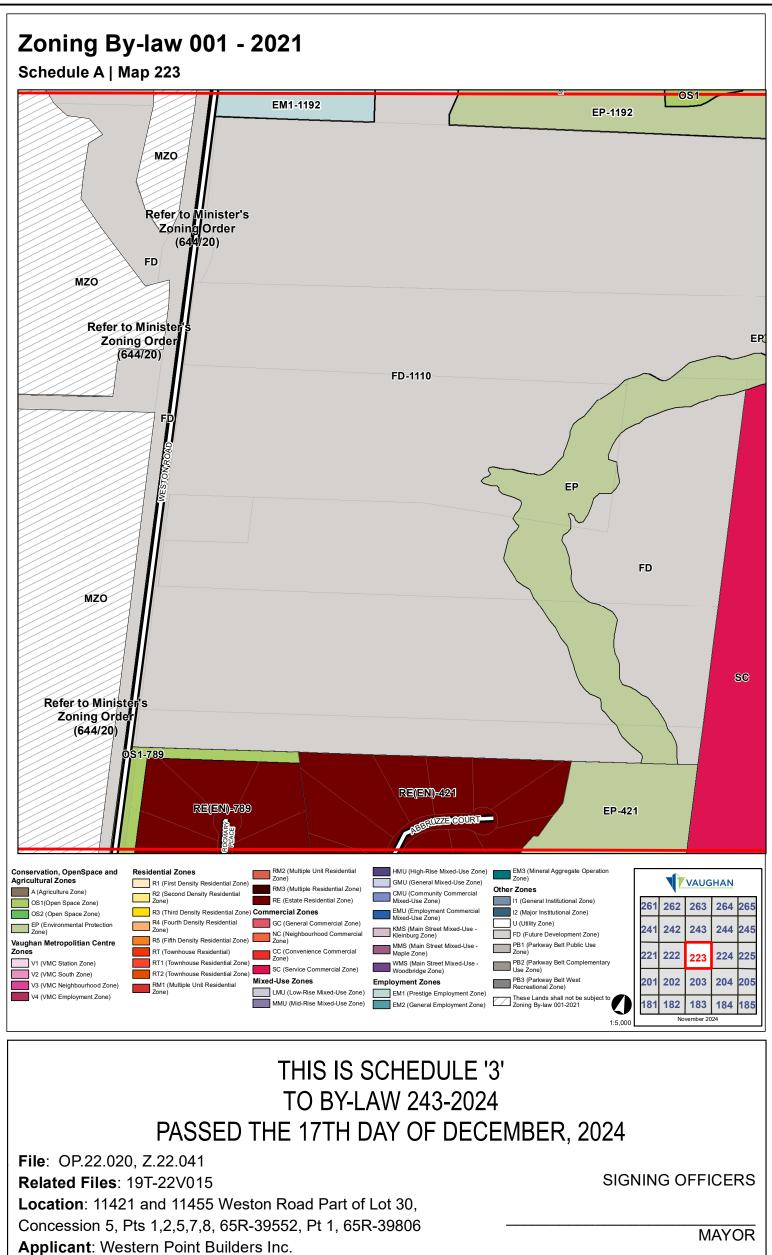
Steven Del Duca, Mayor

Todd Coles, City Clerk

Authorized by Item No. 1 of Report No. 41 of the Committee of the Whole. Report adopted by Vaughan City Council on December 17, 2024. City Council voted in favour of this by-law on December 17, 2024. Approved by Mayoral Decision MDC 017-2024 dated December 17, 2024. **Effective Date of By-Law: December 17, 2024** 

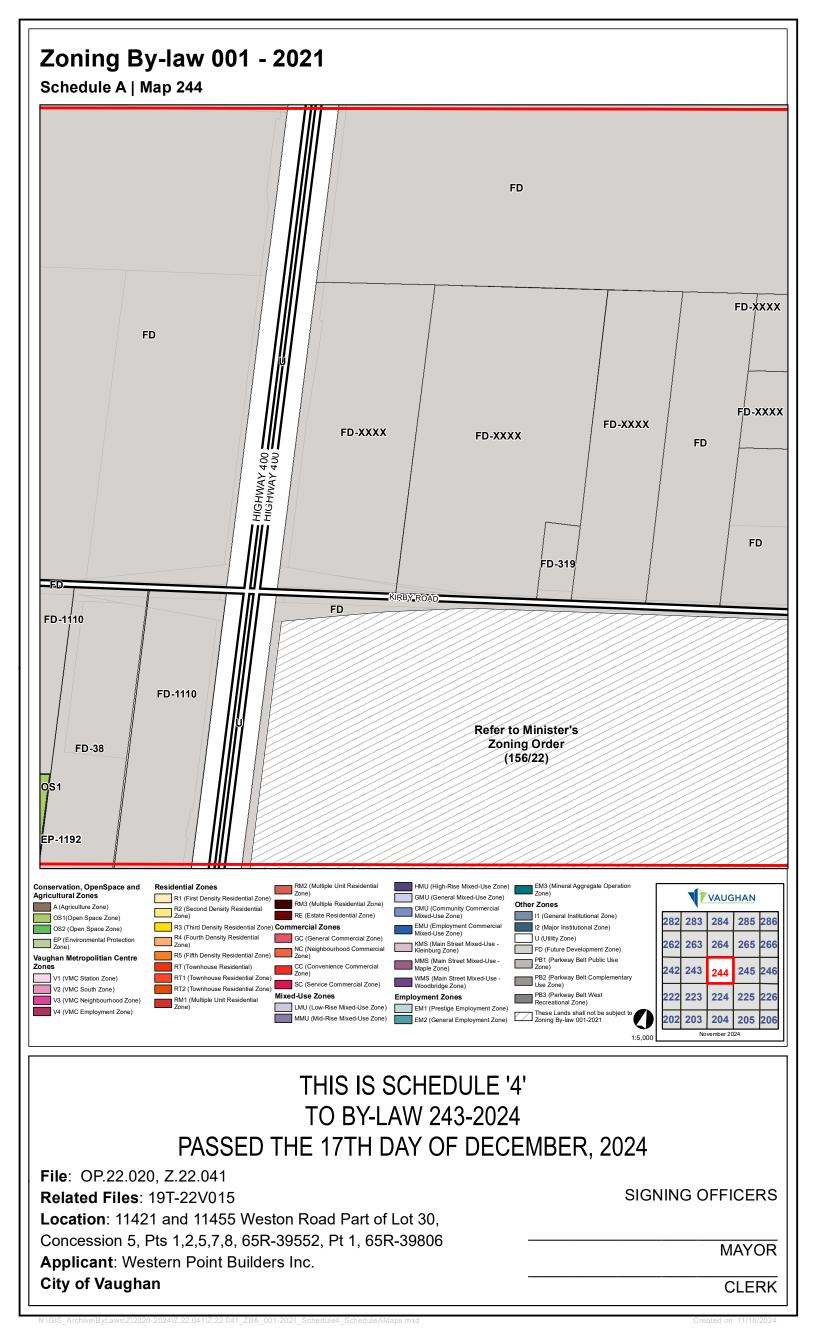






City of Vaughan

CLERK



#### SUMMARY TO BY-LAW 243-2024

The lands subject to this By-law are located east of Weston Road and south of Kirby Road, known municipally as 11421 and 11455 Weston Road, in Part of Lot 30, Concession 5, City of Vaughan.

The purpose of this by-law is to amend the City of Vaughan Zoning By-law 001-2021 to rezone the subject lands from FD Future Development Zone with site-specific Exception 1110, to EMU Employment Commercial Mixed-Use Zone, EM1 Prestige Employment Zone, EP Environmental Protection Zone and OS1 Public Open Space Zone and to add site-specific exceptions to facilitate the proposed Subdivision Plan development.

